

Plan Amendment Application **Pioneer Meadows**

Submitted to City of Sparks
December 26, 2018

Community Services Department Original

Prepared for

BPH | LLC

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Prepared by



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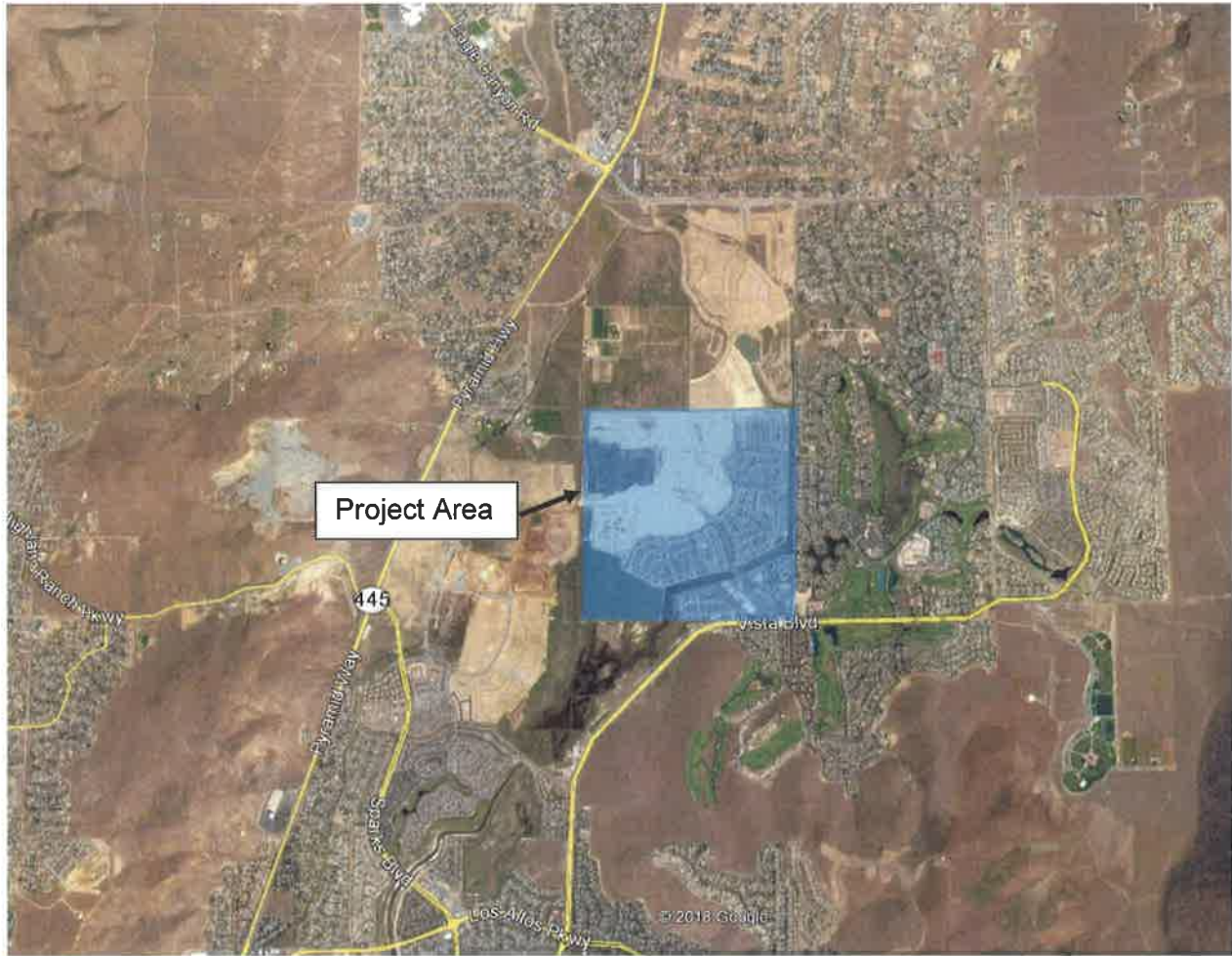
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Vicinity Map

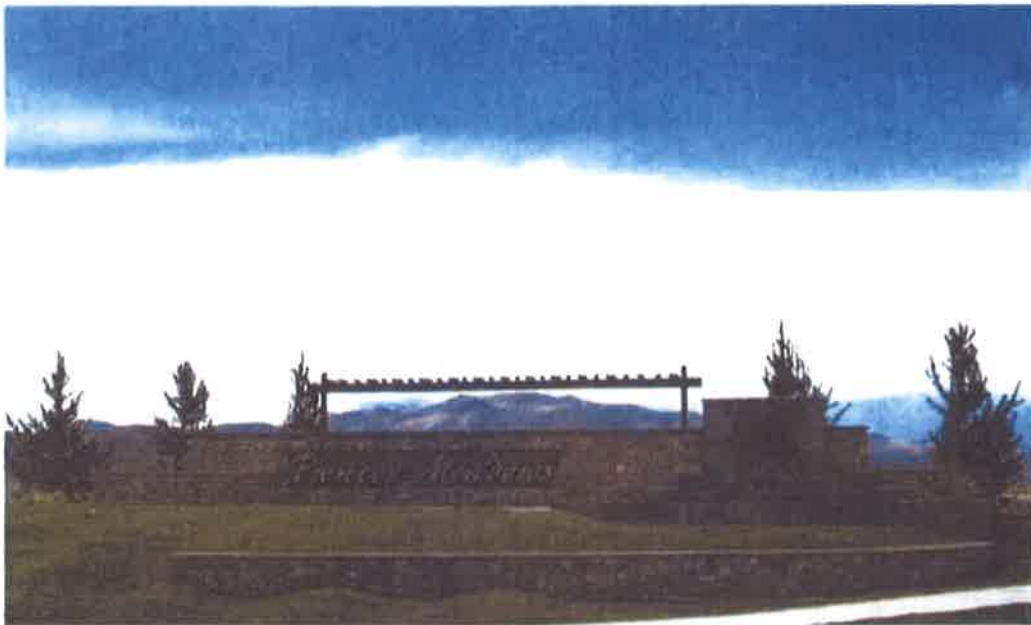
Section 1

Section 2

PIONEER MEADOWS

Development Standards Handbook

Sparks, Nevada



Prepared for:

Pioneer Meadows LLC

Final-2006

Handbook Amendment #3

December 2018

Prepared by:

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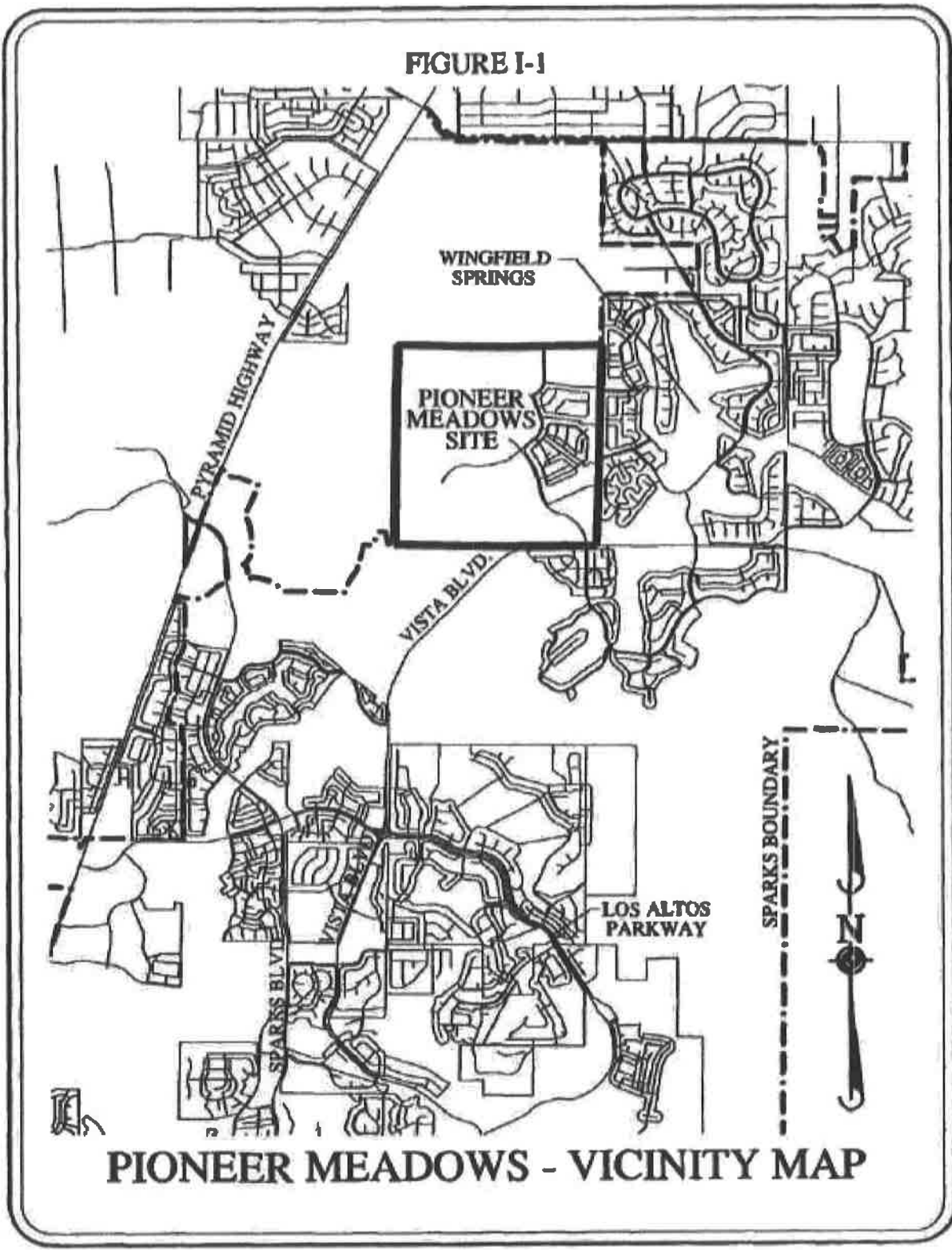
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SECTION I

GENERAL



INTRODUCTION

The purpose of this document is to outline the policies, procedures, developmental standards, goals and objectives of the *Pioneer Meadows* development. This development will encourage flexibility and creativity of design and diversity of building types, common area arrangements and other aspects of land planning, while at the same time preserving public welfare, health and the general intent of the City of Sparks' Master Plan.

The 640 acres known as Pioneer Meadows was annexed into the City of Sparks (A-1-99) and rezoned (Z-1-99) from S (Study) to PD (Planned Development) in 1999. In addition, the Master Plan was amended (MP-1-99) to realign Pace Drive and Kiley ranch Road. This change allowed for a reduction in the General Commercial land use designation from 109 acres to forty-seven (47) acres and an increase the Business Park from forty-seven (47) acres to 109 acres. Also included were a nine (9) acre park site and a ten (10) acre school site as part of the overall 152 acres of Common Area. In 1999, the Pioneer Meadows Planned Development proposed approximately 2,588 new single-family and multiple-family residential units with a maximum overall gross density of 4.04 dwelling units per acre (du/ac).

OWNER

Pioneer Meadows Master Planned Community is owned by ~~Barker Investments~~.—Pioneer Meadows Development, LLC. and Pioneer Meadows Investments, LLC ~~also have interest in this community.~~

APPLICANT/DEVELOPER

The Applicant/Developer for *Pioneer Meadows* is Pioneer Meadows Development, a Limited Liability Company, locally owned and operated. Lennar Reno, LLC is the managing entity for the partnership and all correspondence related to this application shall be addressed to Pioneer Meadows LLC, located at 10315 Professional Circle, Suite 110, Reno, NV 89521, attention Dustin Barker.

GOALS/POLICIES/PROCEDURES

Overall Project Goals

- To help meet the future projected housing needs while maintaining and managing natural resources and open space.
- To create a jobs/housing balance, while minimizing the impacts of development on air quality and the transportation network.

Objectives

- To provide compatibility between land use and transportation.

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Section I-General

- To reduce vehicle miles traveled by providing employment and housing opportunities within proximity to one another.
- To provide a mix of housing types and sizes that reflect demands.
- To integrate the housing to compliment the site's natural environment.
- To retain and provide access to natural open space.
- To provide trails and bike paths within the Common Areas and right-of-ways in and around the site to the neighborhood parks, and connection of the trails to the regional park within Spanish Springs.

PROJECT OVERVIEW

Pioneer Meadows is located within the Northern Sparks Sphere of Influence (NSSOI). The NSSOI Area Plan, adopted by the City of Sparks in October 1991, designates the land uses and densities associated with *Pioneer Meadows*. The land use and densities will not change and will be consistent with the NSSOI Area Plan, except for the following:

- Modification of the land use classification locations to reflect site conditions
- Modifications to the area infrastructure plan to maximize efficiency
- Modify NSSOI to allow for buffers between conflicting land uses
- Modify NSSOI to achieve better traffic flow and arterials access

The 640 acre development will include twenty-five (25) acres of General Commercial (a decrease of twenty-two (22) acres from original Master Plan (MP-1-99), 102 acres of Business Park (a decrease of seven (7) acres from MP-1-99), ninety-one (91) acres of Fifteen (15) dwelling units per acre (du/ac) - Multi-Family (an increase of seven acres from MP-1-99), sixty (60) acres of Ten (10) du/ac - Single Family, 197.8208.4 acres of Four (4) du/ac - Single Family (an increase of 26.4 acres ~~n increase of 15.8 acres~~ from MP-1-99) and 146.7 acres of Common Area (an increase of 7.7 acres from MP-1-99). Please reference to **Figures II-1 through II-4** for land use depictions. In order to keep prospective buyers aware of the surrounding land uses, Pioneer Meadows LLC will supply these individuals with the Project Master Plan (reference to **Figure II-4**).

LAND USE SUMMARY

Four (4) Dwelling Units per Acre (du/ac) - Single Family

The NSSOI Area Plan defines this related land use as Four (4) du/ac, and designates 182 acres as such. The original *Pioneer Meadows* Master Plan (MP-1-99) also designated 182 acres under this land use classification.

The NSSOI Area Plan has Four (4) du/ac dispersed throughout the project area. *Pioneer Meadows* development will concentrate this land use into one area located within the northeast portion of the site. The Four (4) du/ac land use related to this project will permit a land use classification of SFR4 and totals 197.8208.4 acres. The SFR4 land use is discussed further in **Section III - Development Standards and Descriptions**.

Ten (10) Dwelling Units per Acre (du/ac) - Single Family

The NSSOI Area Plan defines this related land use as Ten (10) du/ac, and designates sixty (60) acres as such. Both the 1999 (MP-1-99) and the 2006 (MP06005) *Pioneer Meadows* Master Plan designate sixty (60) acres under this land use classification.

The NSSOI Area Plan has Ten (10) du/ac split into two (2) areas, whereas, both the 1999 (MP-1-99) and the 2006 (MP06005) *Pioneer Meadows* Master Plans modify this land use by combining the two areas into one area, located in the southern middle

Development Standards Handbook for Pioneer Meadows
Section II-Master Plan

sector of the site (proximate to the NSSOI Area Plan location). The Ten (10) du/ac land use related to this project will permit a land use classification of RD10A, discussed further in **Section III - Development Standards and Descriptions**.

Fifteen (15) Dwelling Units per Acre (du/ac) - Multi-Family

The NSSOI Area Plan defines this related land use as Fifteen (15) du/ac, and designates eighty-four (84) acres as such. The original *Pioneer Meadows* Master Plan (MP-1-99) also designated eighty-four (84) acres under this land use classification.

The NSSOI Area Plan has this land use classification located in two (2) locations. *Pioneer Meadows* also proposed this land use in two (2) locations: a thirty (30) acre parcel located along Vista Boulevard and a sixty-one (61) acre parcel located within the west central portion of the project area and is subdivided into three separate villages. The Fifteen (15) du/ac land use classification permits a land use classification of MF15 and MF15A and are discussed further in **Section III - Development Standards and Descriptions**.

General Commercial

The NSSOI Area Plan designates 109 acres as Commercial land use. The locations of this land use category are as follows: a twenty-eight (28) acre parcel located along Vista Boulevard and an eighty-one (81) acre parcel located within the upper middle of the site.

The original Pioneer Meadows Master Plan (MP-1-99) designated forty-seven (47) acres as Commercial land use. The current Master Plan (MP06005) now designates twenty-five acres of Commercial land use. The location is twenty-five (25) acres along Vista Boulevard (split by Hills Drive). The Commercial land use related to this project will permit a land use classification of GC and is discussed further in **Section III - Development Standards and Descriptions**.

Business Park

The NSSOI Area Plan defines this related land use as Business Park and designates forty-seven (47) acres as such. *The original Pioneer Meadows* Master Plan (MP-1-99) designated 109 acres under this land use classification.

The acreage associated with the Business Park has been relocated within *Pioneer Meadows*. Moving the Business Park location provides an improved buffer between the residential and commercial land uses and provides better access between the Business Park(s) and the arterial roads. The land use for the Business Park has been separated as follows: a fourteen (14) acre parcel located north of Vista Boulevard (BP-A) and an eight-eight (88) acre parcel located in the northwest section (BP) of the project area. The Business Park land use classification is discussed further in **Section III - Development Standards and Descriptions**.

**Development Standards Handbook for Pioneer Meadows
Section II-Master Plan**

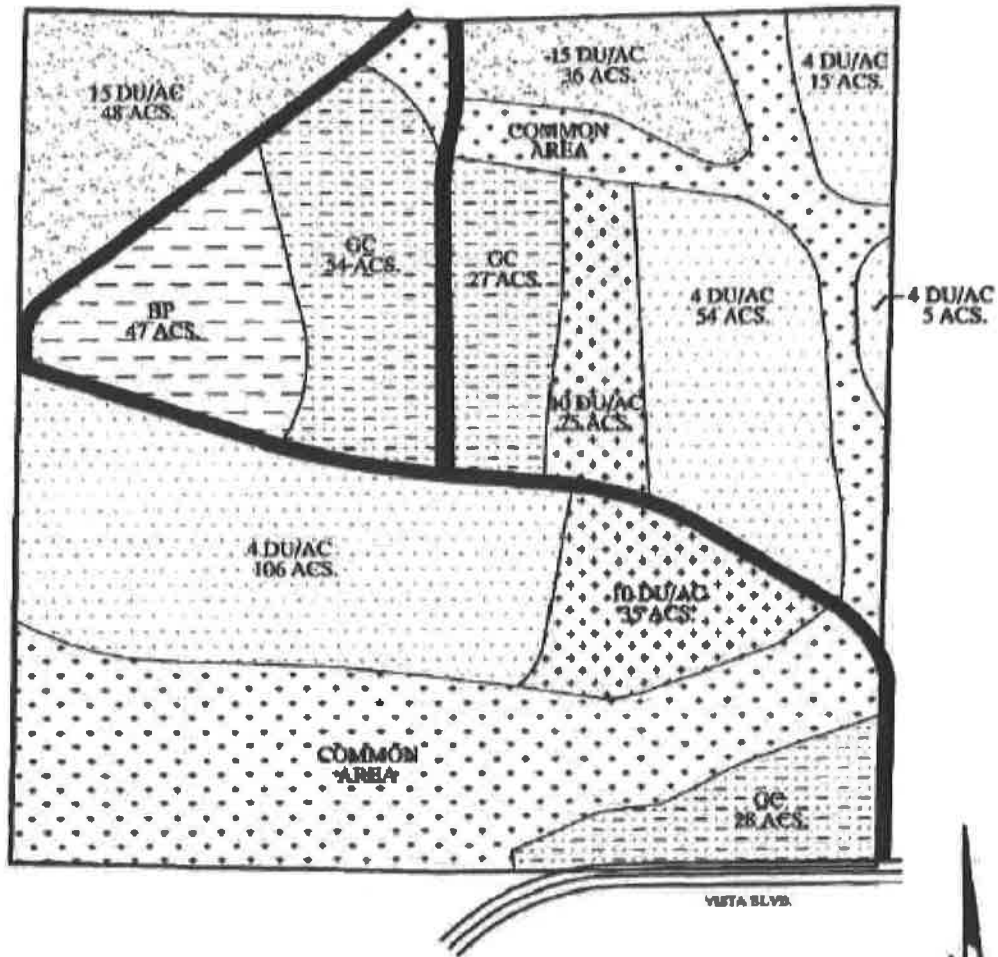
Common Area







A planned development requires 20% of the total acreage to be Common Area. *Pioneer Meadows* is setting aside 146.7 acres (22.8192%) as Common Area.

In addition to the 146.7 acres of Common Area, ~~seven and one-half (7.5) nine (9)~~ acres of parks ~~and an eight and a half (8.5) acre school site~~ are incorporated into *Pioneer Meadows*. The parks are strategically located throughout Pioneer Meadows in three (3) locations, ~~all~~ within walking distance from the various residential villages, ~~any of the residences~~. The ~~three~~ parks proposed include ~~a four and one half (4.5) acre park contiguous to the school site~~, a two and one half (2.5) acre park located in the southeastern portion of within the SFR4 area and a ~~five~~three (3) acre park centrally located along the western project boundary adjacent to the within MF15 within which is contiguous to the large Common Area south of Wingfield Hills Drive at the southwest corner of the site. All of the parks ~~(excepting the school site park)~~ will be accessible by a trail system (refer to Parks and/or Common Area in **Section III - Development Standards and Descriptions.**)

Table II-1 describes the land use breakdown for *Pioneer Meadows* and compares it to the NSSOI Area Plan:

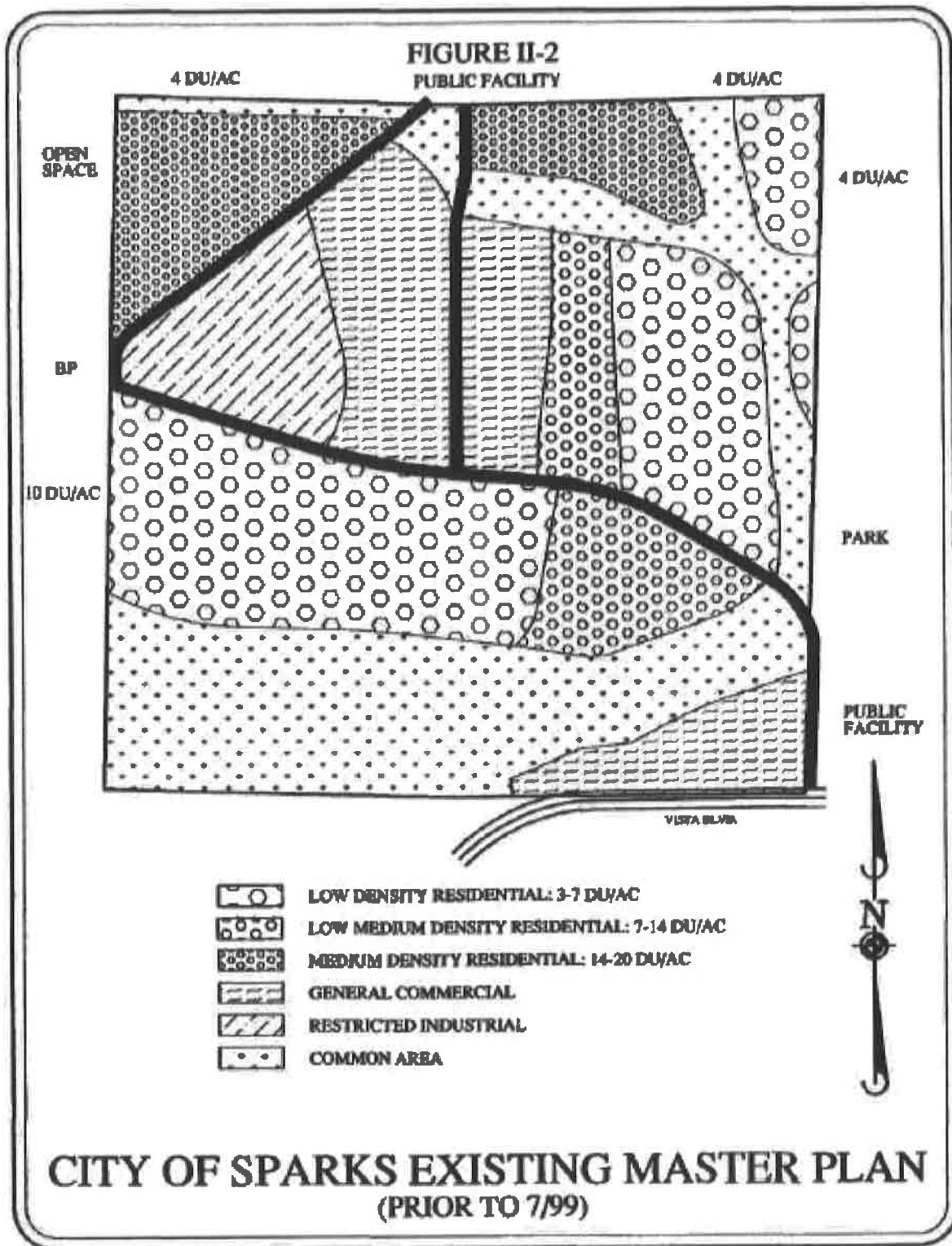
FIGURE II-1

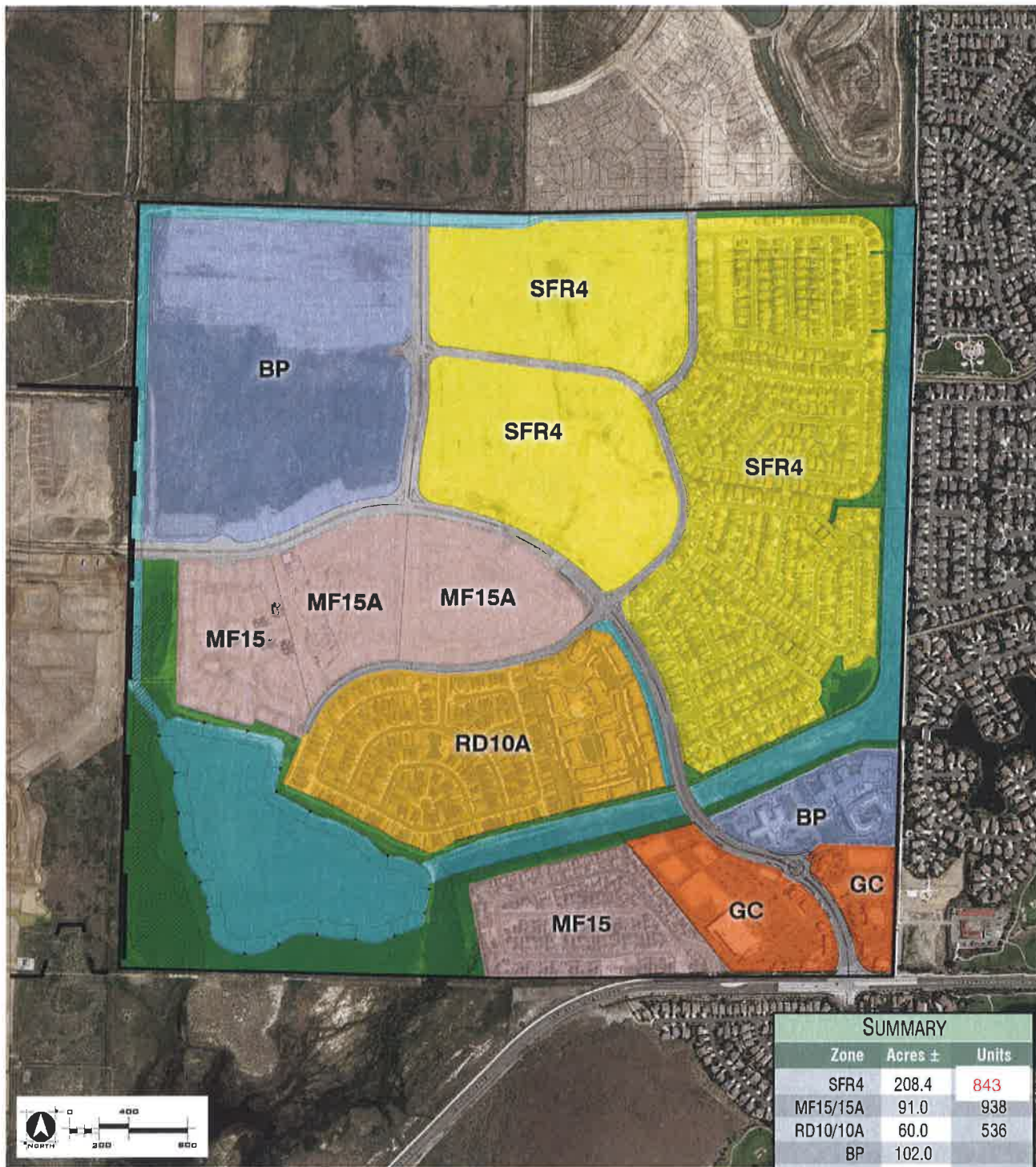


-  SINGLE FAMILY (4 DU/AC)-728 UNITS/182 ACRES
-  SINGLE FAMILY (10 DU/AC)-600 UNITS/60 ACRES
-  MULTI-FAMILY (15 DU/AC)-1260 UNITS/84 ACRES
-  COMMERCIAL (GC)-109 ACRES
-  BUSINESS PARK (BP)-47 ACRES
-  COMMON AREA 158 ACRES

TOTAL AREA - 640 ACRES

CITY OF SPARKS NSSOI DESIGNATIONS





| SUMMARY | | |
|----------|---------|-------|
| Zone | Acres ± | Units |
| SFR4 | 208.4 | 843 |
| MF15/15A | 91.0 | 938 |
| RD10/10A | 60.0 | 536 |
| BP | 102.0 | |
| GC | 25.0 | |
| Common | 144.6 | |
| Parks | 9.0 | |

PIONEER MEADOWS PROJECT MASTER PLAN

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 Reno, NV 89502 Fax: 775.823.4068

MASTER PLAN AMENDMENTS

The 1999 Pioneer Meadows Master Plan included an increase in Business Park from forty-seven (47) acres to 109 acres, a decrease in General Commercial from 109 acres to forty-seven (47) acres and realignment of the intersection of Hills Drive and Kiley Drive. The realignment of the intersection was completed in conjunction with the RTC, the City of Sparks' Public Works Department, the City of Sparks' Planning and Community Development Department, Pioneer Meadows LLC and a representative of the Kiley Ranch property.

The 2006 Pioneer Meadows Master Plan included twenty-five (25) acres of General Commercial (a decrease of twenty-two (22) acres from original Master Plan (MP-1-99), 102 acres of Business Park (a decrease of seven (7) acres from MP-1-99), ninety-one (91) acres of Fifteen (15) dwelling units per acre (du/ac) - Multi-Family (an increase of seven acres from MP-1-99), sixty (60) acres of Ten (10) du/ac - Single Family, 197.8 acres of Four (4) du/ac - Single Family (an increase of 15.8 acres from MP-1-99) and 146.7 acres of Common Area (an increase of 7.7 acres from MP-1-99).

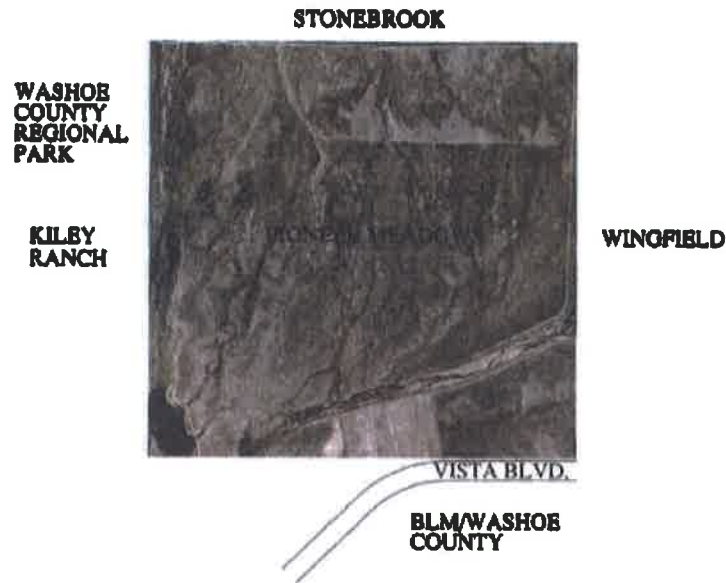
GENERAL SITE AND SURROUNDINGS

Pioneer Meadows is 640 acres located in the heart of the Spanish Springs Valley, Section 11 Range 20 East, Township 20 North, Mount Diablo Meridian (M.D.M.). The site is bordered on the east by Wingfield Springs, on the north by Stonebrook, on the west by Kiley Ranch and the Washoe County Regional Park and on the south by Vista Boulevard.

Pioneer Meadows and Wingfield Springs will be separated on the eastern border by a minimum of 140 feet due to a regional drainage facility (open channel) that services the Spanish Springs Valley. A trail system developed within *Pioneer Meadows* will connect Wingfield Springs to the Washoe County Regional Park.

Pioneer Meadows is a flat site with a gradient of not more the 0.003 feet per foot (ft/ft), which does not meet the threshold established in the City of Sparks' Hillside Ordinance for hillside development.

FIGURE II-5

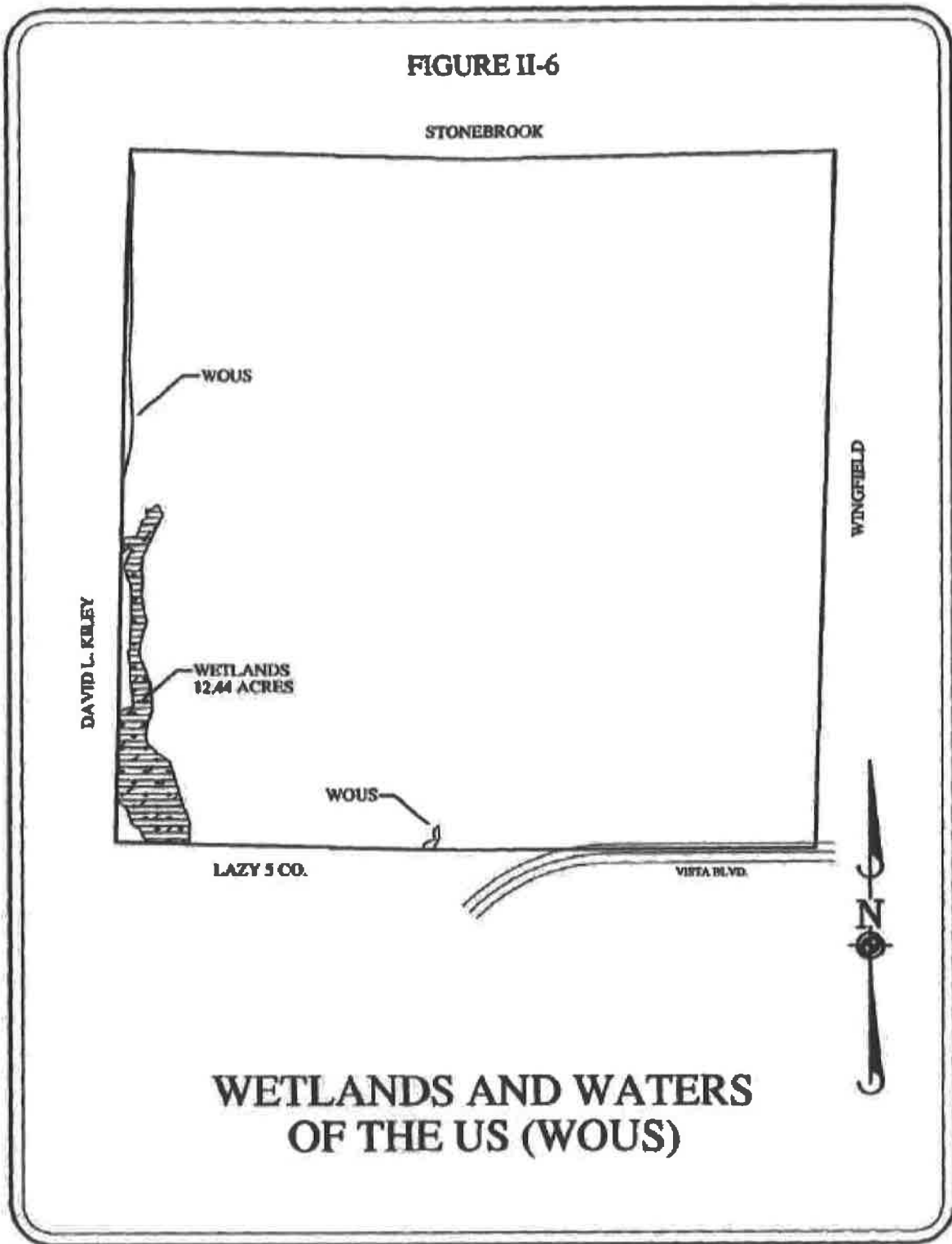


WETLANDS

Huffman and Associates were employed to perform a Wetlands Delineation on the *Pioneer Meadows* site. These reports were reviewed and verified by the Army Corps of Engineers. The findings reported in the delineation detail approximately 12.44 acres of wetlands and two locations where Waters of the United States (WOUS) are applicable (reference to **Figure II-6**). These wetland areas will be incorporated into the Common Area and trail system. These reports are included in *The Supplement to the Development Standards Handbook for Pioneer Meadows*.

CULTURAL RESOURCES

Kautz Environmental Consultants, Inc. were employed to conduct an archaeological reconnaissance of the *Pioneer Meadows* site. This study was completed in conformance with Federal Standards (Bureau of Land Management, Nevada Guidelines) and will satisfy the local reviewing agencies (Washoe County and the Nevada State Historical Preservation Office [SHPO]). The results of the study revealed one regionally significant site, KEC-38-1. A treatment plan was prepared, accepted and executed in conjunction with the SHPO. These reports are included in *The Supplement to the Development Standards Handbook for Pioneer Meadows*.



PROCEDURES

Once the *Pioneer Meadows* application is approved, a Development Agreement will be entered into between Pioneer Meadows LLC and the City of Sparks that will incorporate this Handbook. The review and approval process for subsequent submissions will be subject to the following guidelines:

Definitions

- **Administrators:** The City of Sparks' Public Works Director and the City of Sparks' Community Development Director shall be the Administrators, hereinafter referred to individually or collectively as the "Administrator" of this process. The Administrator shall have the principal authority to interpret and make decisions based on the plans, standards and guidelines contained herein which are in accordance with the scope of the responsibilities granted to them in Titles 15, 17 and 20 of the City of Sparks' Municipal Code.
- **Applicant/Developer:** Pioneer Meadows LLC, or assignee, shall be the Applicant/Developer, and will be hereinafter referred to as "Applicant" or "Developer" during this process. All communication between the City of Sparks and the Applicant or Developer must be directed to Pioneer Meadows LLC.
- **Builder:** The person or entity which will construct final improvements with respect to a parcel of the property.

Design/Flexibility

The development standards and design guidelines contained herein are intended to depict the general nature and relative intensity of the residential and non-residential development at *Pioneer Meadows*. Sufficient flexibility shall be allowed to permit detailed planning and design at the time of actual development. The configuration and acreage of development parcels and phases may be altered from what is shown on the Master Land Use Plan to accommodate detailed site conditions. Land uses may also be transferred between development parcels, providing this transfer does not conflict with the standards described in this Handbook. The land use "*transfers*" reflect a market driven project that has the flexibility to adjust to the possible fluctuations that may occur in the future market conditions.

Omissions

When issues arise that are not covered in this Handbook, the provisions and definitions of the City of Sparks' Zoning Ordinance and Subdivision Regulations in effect at the time of application shall apply.

Minor Deviations

The Administrator shall have the authority at his/her discretion to administratively approve minor deviations in the plans, standards and guidelines as requested by the Developer prior to the submission of a final development plan for each phase. Minor

**Development Standards Handbook for Pioneer Meadows
Section II-Master Plan**

deviations include such items as parcel configurations, parcel sizes, irregular lots and special setback conditions (up to 20% of the required setbacks).

Major Deviations

Major deviations will be defined as any deviation that changes the intent of the approved Handbook, such as zoning changes, deviations greater than twenty percent (20%) to the established development standards and/or movement of land use classifications which are not in conformance with the development standards described within this Handbook. Major deviations will be handled as amendments to this Handbook (reference to **Amendments**, this section).

Appeals

The Applicant may appeal any decision made by the Administrator to the City of Sparks' Planning Commission. The Planning Commission shall hear the appeal and either affirm, modify or reverse the determination of the Administrator. Any decision made by the Planning Commission may be appealed to the City of Sparks' City Council.

Amendments

Any amendments made to the approved Handbook will be processed through the City of Sparks' Planning Commission and City Council. Prior to Planning Commission action the applicant shall hold a neighborhood meeting. Noticing of such a meeting shall conform to the Noticing requirements stated in **Section V** of this Handbook. Amendment noticing shall be from the perimeter of the area proposed for amendment. The City of Sparks may amend this Handbook subject to the provisions of the Development Agreement.

Site Plan Review and Special Use Permits

It is the intent of this Handbook to assure the Developer that the design requirements for *Pioneer Meadows* are understood and agreed upon between all parties involved, including the City of Sparks. This will give the Developer a high level of comfort, accurate costs of each of the developments and specific criteria for determining the cost of improvements related to the project. This Handbook describes specific criteria for the development of each land use. Defined in each land use are particular uses which will require a **Site Plan Review** or a **Special Use Permit**. Within the criteria of these review processes, condition(s) may be added to specific project. These conditions will be limited to the necessary infrastructure to complete the individual project. All condition(s) related to these reviews will conform to the standards set forth in this Handbook.

The Developer shall begin the Tentative Map process by preparing the Tentative Map Application. Following application submittal, the Community Development Department will process, review and distribute materials in preparation for the Tentative Map review. This may be accomplished and is precedent to the Planning Commission Meeting. At this stage, the Tentative Map may be approved, approved subject to conditions, or denied. If approved or approved subject to conditions, a final map is to be submitted that meets the conditions of approval of the Tentative map and that is in compliance with the tentative map. If denied, the Developer may choose to appeal to the district court.

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Section II-Master Plan

All previously approved subdivisions, including but not limited to Villages 1-4, 5A, 6A, BP-A (the fourteen (14) acre Business Park, and the thirty (30) acre MF15 subdivision, are not subject to the conditions of the Tentative Map procedure. For further detail or information, please reference **Section V – Plan Processing and Submittals**.

SECTION III

DEVELOPMENT STANDARDS

AND

DESCRIPTIONS

**Development Standards Handbook for Pioneer Meadows
Section III-Development Standards and Descriptions**

INTRODUCTION

Pioneer Meadows identifies a series of residential and non-residential development parcels. The configuration of the parcels depicted on the Master Plan may vary in accordance with this Handbook. The following development standards are intended to direct the nature, intensity and quality of residential and non-residential development uses with *Pioneer Meadows*. These standards shall be used by the Administrator in determining the acceptability of the individual final development plans submitted for each phase of development. Each standard describes **Permitted Uses**, Uses Requiring a **Site Plan Review** and Uses Requiring a **Special Use Permit**. The procedure by which plans are processed and submitted for each of these uses is described in detail in **Section V-Plan Processing and Submittals**.

SFR4

Description

SFR4 encompasses ~~197.8~~208.4 acres at four (4) dwelling units per acre (du/ac), yielding a total of ~~791843~~ units. The ~~791843~~ units have been separated into six individual Villages of approximately 120 units per Village. Some Villages, when evaluated on a stand-alone basis, may contain higher densities, but in no case will the aggregate total exceed ~~791-843~~ units on a total of ~~208.4 acres~~197.8 acres. SFR4 is serviced by unloaded (no driveways) residential collectors feeding into two arterials which service the entire project (reference to **Section VII - Infrastructure and Public Services**).

The minimum lot size associated with SFR4 is 6,000 square feet (SF). Minimum side yard setbacks are seven-and one-half feet (7.5') and exterior side yard setbacks are fifteen feet (15') with a combined total of twenty feet (20'). These setbacks give the option to move the structure from side to side to accommodate recreational vehicle parking on the garage side of the structure, if desired. Please reference to **Table III-1**.

**TABLE III-1
SFR4 CLASSIFICATION STANDARDS**

| | SIDE YARD | FRONT YARD | REAR YARD | EXTERIOR SIDE YARD | INTERIOR LOTS | CORNER LOTS |
|-----------------------------|----------------------------------|------------------------------------|---|---------------------------|----------------------|--------------------|
| PIONEER MEADOWS SFR4 | 7.5' MINIMUM | 15' TO HOUSE OR SIDE LOADED GARAGE | 20' | 15' | 6,000 SF | 6,500 SF |
| | 15' MINIMUM – EXTERIOR SIDE YARD | 10' TO PORCH | 15' WHEN ADJACENT TO OPEN SPACE (PUBLIC OR PRIVATE) | | | |
| | | 20' TO GARAGE | | | | |

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Section III-Development Standards and Descriptions

The streets within SFR4 are standard City of Sparks' sections; fifty-five foot (55') right-of-way and forty-six (46') right-of-way for cul-de-sacs.

Gated Communities with Private Streets Are Permitted

A private gated community, with adequate units to support items such as private streets, a community center and private recreational facilities will be granted the flexibility to reduce street right-of-ways to accommodate a twenty-nine foot (29') travel way (fire access requirements) with adequate off street parking and pedestrian access to support the reduced right-of-ways.

Permitted Uses

- Single family dwellings of a permanent nature and their accessory buildings and uses.
- Gated community with private streets
- In-home child care for the number of children one care giver may care for in accordance with Washoe County's Rules and Regulations for child care facilities.
- Home occupations.
- Accessory structures.
- Model home complex with sales office for the same Village.
- Sales office trailers for same Village.
- Temporary construction yards for the same Village.

Uses Requiring a Site Plan Review

- Recreational facilities (for the use of residents of the immediate neighborhood).

Uses Requiring a Special Use Permit

- Above ground public utility structures.

Lot Design Criteria

- Minimum corner lot size – 6,500 SF.
- Minimum lot size - 6,000 SF.
- Minimum lot frontage - Sixty feet (60') at building setback line.
- Side yard setback - Minimum seven and one-half feet (7.5').

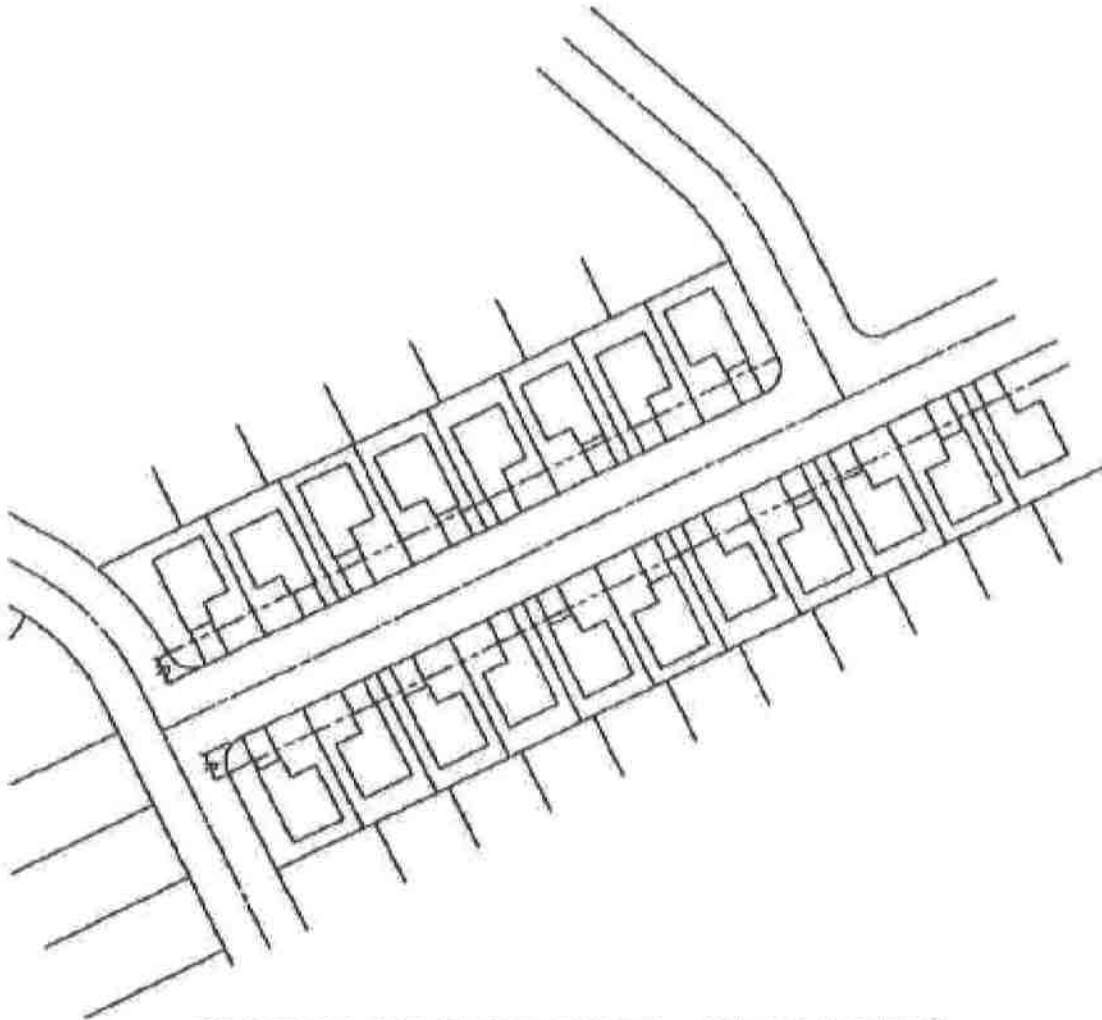
Development Standards Handbook for Pioneer Meadows
Section III-Development Standards and Descriptions

- Front yard setback - Twenty feet (20') to garage.
Fifteen feet (15') to house or side loaded garage.
Ten feet (10') to porch.
- Rear yard setback - Twenty feet (20') or fifteen feet (15') when adjacent to pen space (public or private).
- Maximum building height - Thirty feet (30').

Variable Setbacks

Variable setbacks will be applicable when four (4) or more contiguous structures are located on a linear street. When this criteria is met or exceeded, the homes shall be staggered such that the front setback of any two (2) dwellings shall vary by not less than three feet (3') (please reference to **Figure III-1**). The minimum front yard setbacks listed above shall be maintained.

FIGURE III-1



VARIABLE FRONT SETBACKS

WHERE THE CENTER LINE OF THE STREET DOES NOT VARY AND FOUR (4) OR MORE STRUCTURES LINE THE STREET, STRUCTURES SHALL BE STAGGERED SUCH THAT THE FRONT SETBACK OF ANY TWO (2) STRUCTURES SHALL VARY BY NOT LESS THAN THREE FEET (3'). THE INTENT IS TO AVOID THE APPEARANCE OF A SOLID WALL OF BUILDING FRONTS ALONG THE STREET FRONTAGE.

**Development Standards Handbook for Pioneer Meadows
Section III-Development Standards and Descriptions**

SFR6

SFR6 is an acceptable residential use within all residential zones. SFR6 is a small lot single family designation that allows for either public or private streets.

The minimum lot size associated with SFR6 is 3,500 square feet (SF). Minimum side yard setbacks are three and one-half feet (3.5'). Please reference to **Table III-2**.

**TABLE III-2
SFR6 CLASSIFICATION STANDARDS**

| | SIDE YARD | FRONT YARD | REAR YARD | EXTERIOR SIDE YARD | INTERIOR LOTS | CORNER LOTS |
|-----------------------------|---|--|---|---|----------------------|--------------------|
| PIONEER MEADOWS SFR6 | 3.5' MINIMUM 7' BETWEEN STRUCTURES NO ENCROACHMENT WITHIN THIS SERBACK IS ALLOWED INCLUDING, BUT NOT LIMITED TO EAVES, CHIMNEYS, AND WINDOWS | 10' TO STRUCTURE, 5' TO COVERED OR UNCOVERED PORCHES 20' TO GARAGE | 15' TO STRUCTURE 10' TO PORCHES (COVERED OR UNCOVERED) | 8.5' TO STRUCTURE 5' TO PORCHES (COVERED OR UNCOVERED) | <u>3</u> 4,500 SF | <u>4</u> 5,000 SF |

Gated Communities with Private Streets

SFR6 is conducive to a private gated community, with adequate units to support items such as private streets, a community center and private recreational facilities. Such a community will be granted the flexibility to reduce street right-of-ways to accommodate a twenty-six foot (26') travel way (fire access requirements) with adequate off street parking and pedestrian access to support the reduced right-of-ways.

Permitted Uses

- Single family residential.
- Gated community with private streets
- Temporary and permanent model home complexes and sales offices.
- Sales trailers for the same Village.
- Temporary construction yards.

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Uses Requiring a Site Plan Review

- Structures over thirty feet (30') in height.
- Recreational facilities (for the use of residents of the immediate neighborhood).

Uses Requiring a Special Use Permit

- Above ground public utility structures.

Lot Design Criteria

- Minimum lot size – 3,500 SF.
- Minimum lot frontage – Forty feet (40') on linear streets, twenty feet (20') on non-linear streets.
- Side yard setback - Minimum three and one-half feet (3.5').
- Exterior and corner side yard setback – Minimum ~~ten-eight and one-half~~ eight and one-half feet (8.5') to structure and five (5') to covered or uncovered porch.

Note: No Encroachment within this side setback is allowed including, but not limited to eaves, chimneys, and windows.

- Front yard setback - Twenty feet (20') to garage.
Ten feet (10') to structure.
Five feet (5') to covered or uncovered porch.
- Rear yard setback - Fifteen feet (15') to structure.
Ten feet (10') to covered or uncovered porch.
- Maximum building height - Thirty feet (30').

Variable Setbacks

Variable setbacks will be applicable when four (4) or more contiguous structures are located on a linear street. When this criteria is met or exceeded, the homes shall be staggered such that the front setback of any two (2) dwellings shall vary by not less than three feet (3') (please reference to **Figure III-1**). The minimum front yard setbacks listed above shall be maintained.

RD10A

Description

RD10A encompasses sixty (60) acres at ten (10) dwelling units per acre (du/ac), yielding 600 total units. This area is serviced by an unloaded (no driveways) residential collector which feeds into the arterial that services the entire development. The site is bounded on the north and east by an arterial and a residential collector, ~~respectively, with~~ common area ~~on to~~ the south and west ~~and a three (3) acre park located in the southwest portion of the site~~. Access to the exterior trails that border the southern portion of the site will be provided within each development.

RD10A will be developed with multi-family or single family uses, but **in no case will the overall densities on this sixty (60) acres exceed a unit count of 600 units (ten (10) du/ac)**.

With the submittal of the first development defining the first uses, the developer will determine the remaining parcel configurations (subject to change) and preliminary uses (if necessary), illustrating traffic circulation patterns, points of connections and conformance with this Handbook within the sixty (60) acres designated as RD10A.

Permitted Uses

- Single family residential.
- Multi-family residential (not in excess of 15 du/ac).
- Gated communities with private streets.
- Cluster housing (reference SFR10 section).
- Neo traditional residential (reference NT section).
- Temporary and permanent model home complexes and sales offices.
- Sales trailers for the same village.
- Temporary construction yards.

Uses Requiring a Special Use Permit

- Above ground public utility structures.
- Multi-family residential and apartment developments not requiring a tentative map.

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Development Criteria

- Minimum development size - Ten (10) acres
- Minimum lot frontage - Reference individual classifications.
- Side yard setback - Reference individual classifications.
- Front yard setback - Reference individual classifications.
- Rear yard setback - Reference individual classifications.
- Maximum building height - Reference individual classifications.
- Rear yard abutting main arterial - Reference individual classifications.
- Rear yard abutting residential collectors - Reference individual classifications.

Fencing

The fencing around the perimeter of each development (if more than one) will be consistent throughout the development of the RD10A. Please reference each individual classification and **Section IV - Design Standards**.

SFR10

Description

SFR10 is an acceptable residential use within RD10A. SFR10 is cluster type housing units with a maximum of six (6) units per cluster or a minimum of two (2) units per cluster serviced by one (1) private driveway (reference to **Figure III-2**) serviced by either the City of Sparks' standard street sections or a private street (reference to **Section VII-Infrastructure and Public Services-Figures VII-11 through VII-14**).

The average lot size is 3000 square feet (SF) determined by calculating the area of the individual cluster and dividing by the number of units associated with the block. Minimum distance between structures is seven feet (7') and each unit will have a minimum of 680 SF of exterior area, not including the driveway (reference to **Figures III-2 through III-5**).

General Criteria

All of the following items will be addressed in detail in the Covenants, Conditions and Restriction's (CC&R's) for this development:

- All front yards within the clusters and exterior side yards located along the right-of-way shall be landscaped by the builder (reference to **Figure III-2**).
- The above mentioned landscaped areas shall be maintained by the Homeowners' Association (reference to **Figure III-2**).
- All access and maintenance easements associated with the clusters will be addressed on the individual Tract Maps.
- Parking on the driveways within the individual clusters will be prohibited (reference to **Figure III-5**).

Gated Communities with Private Streets

SFR10 is conducive to a private gated community, with adequate units to support items such as private streets, a community center and private recreational facilities. Such a community will be granted the flexibility to reduce street right-of-ways to accommodate a twenty-nine foot (29') travel way (fire access requirements) with adequate off street parking and pedestrian access to support the reduced right-of-ways.

Permitted Uses

- Single family dwellings of a permanent nature and their accessory uses.
- Gated community with private streets.
- Model home complexes with sales office for the same Village.

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- Sales office trailers for the same village.
- Temporary construction yards for the same Village.
- Accessory uses.
- Home occupations.

Uses Requiring a Site Plan Review

- Structures over thirty feet (30') in height.
- Any new multi-family residential and apartment development under 110 units not requiring a tentative map.
- Recreational facilities (for the use of the residents of the immediate neighborhood).

Uses Requiring a Special Use Permit

- Above ground public utility structures.
- Any new multi-family residential and apartment development of 110 units or more not requiring a tentative map.

Lot Design Criteria (reference to Figure III-3)

- Minimum lot size - 2770 SF.
- Minimum average lot size per cluster - 3000 SF.
- Minimum usable area - 680 SF (per unit).
 - Usable area adjacent to or access from - Family room, living room or kitchen.
- Maximum units (lots) per cluster shall be six (6) serviced by one private driveway.

Cluster Setbacks (reference to Figure III-4)

- Rear - Zero feet (0').
- Front - Zero feet (0').
- Side - Zero feet (0').
- Exterior side yard - Ten feet (10').
- Rear property line abutting main arterials - Thirteen feet (13').
(reference to **Figure III-4**)

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- Rear property line abutting residential collectors - seven feet (7').
(reference to **Figure III-4**)

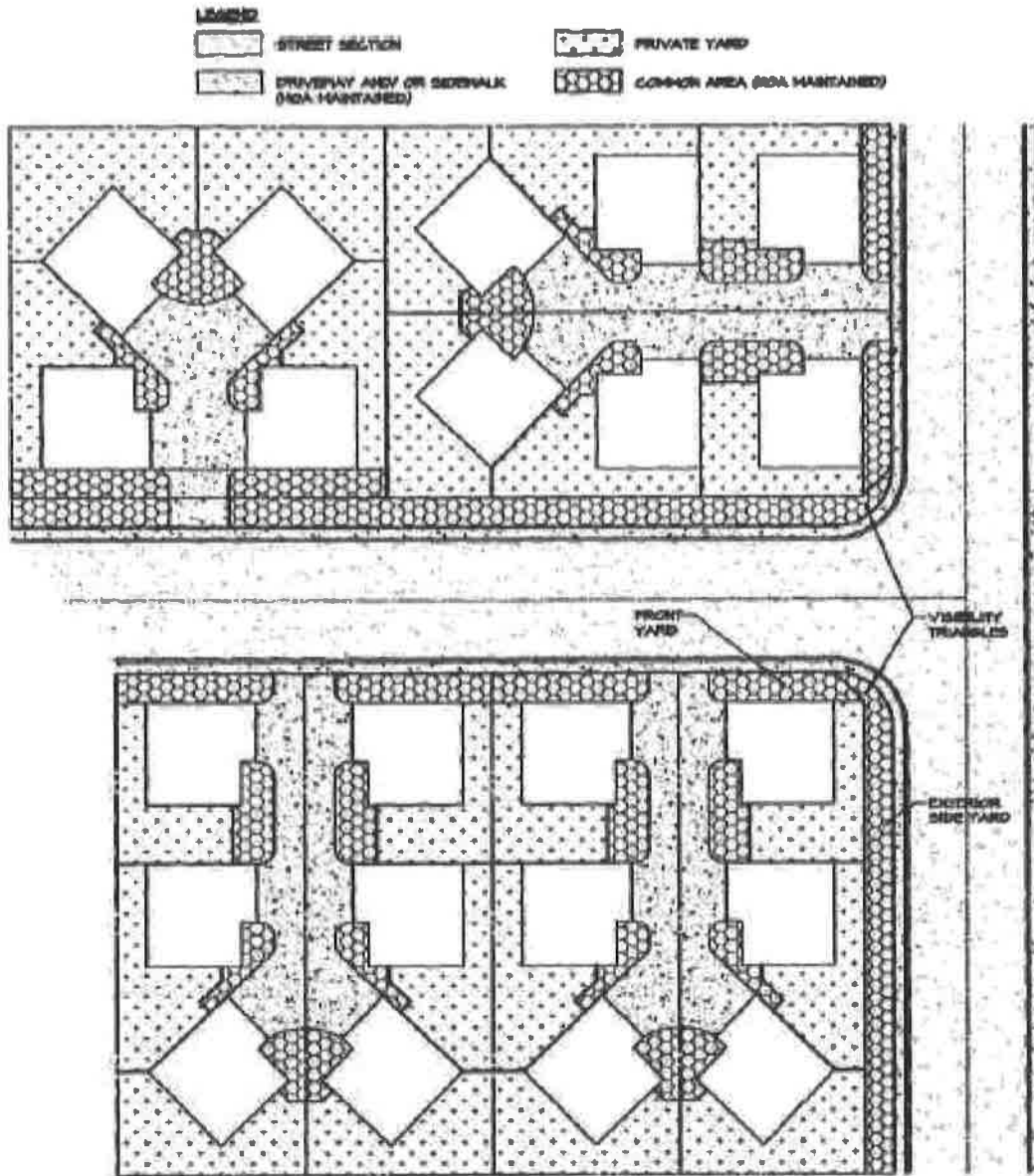
Interior Cluster Design Criteria (reference to Figure III-3)

- Minimum distance between buildings - Seven feet (7').
- Minimum rear yard setbacks - Ten feet (10').
- Minimum driveway width - Sixteen feet (16').
- Minimum Shared driveway width – Twenty feet (20')
(reference to **Figure III-3**)
- Minimum side yard setbacks - Zero feet (0').
- Minimum setbacks from driveway shall be less than or equal to six feet (6') or greater than or equal to twenty feet (20').
- Maximum building height - Thirty feet (30').
- Attached multi-family requires a minimum of 20% landscape area. Half (50%) of the landscape area may include: public areas (plazas, recreational amenities, etc.), utilizing decorative hardscape materials (pavers, stamped and colored concrete, etc.)

Parking Standards

- Private streets twenty-nine feet (29') in width will provide off street parking. All units will provide for two (2) parking spaces within an individual garage. Additional guest parking will be required at a count of one (1) per every two (2) units. Parking within the right-of-way will not be included. Tandem parking is allowed on lots with a minimum eighteen foot (18') drive.
- All vehicles will exit the clusters forward. Turning radii will be supplied with the submittal of each development to demonstrate this movement (exception; when a cluster is one (1) unit).

FIGURE III-2

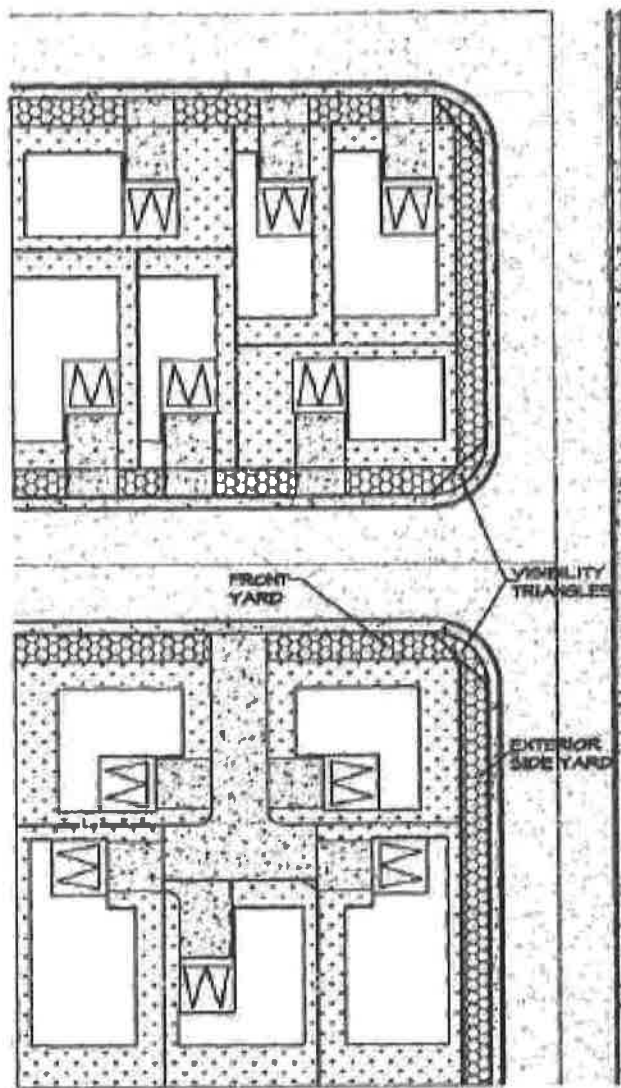


TYPICAL LAYOUT - SFR10
CLUSTER HOUSING

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Section III-Development Standards and Descriptions

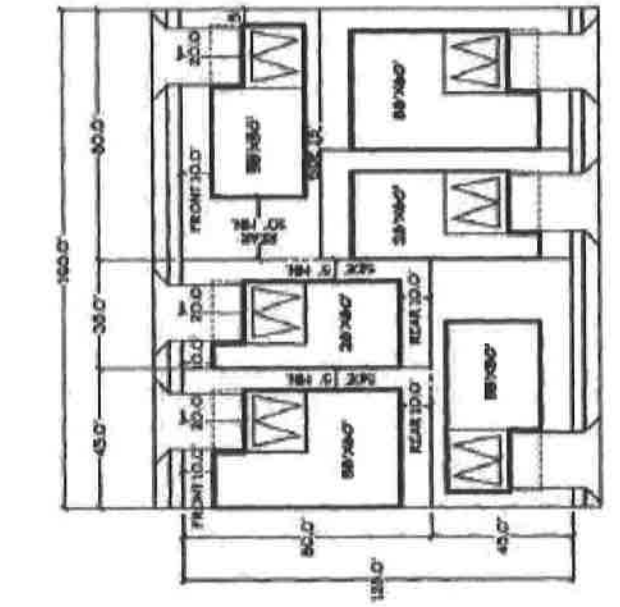
FIGURE III-3

- | | |
|---|--|
|  STREET SECTION |  PRIVATE YARD |
|  DRIVEWAY AND/ OR SIDEWALK |  COMMON AREA (NOT MAINTAINED) |

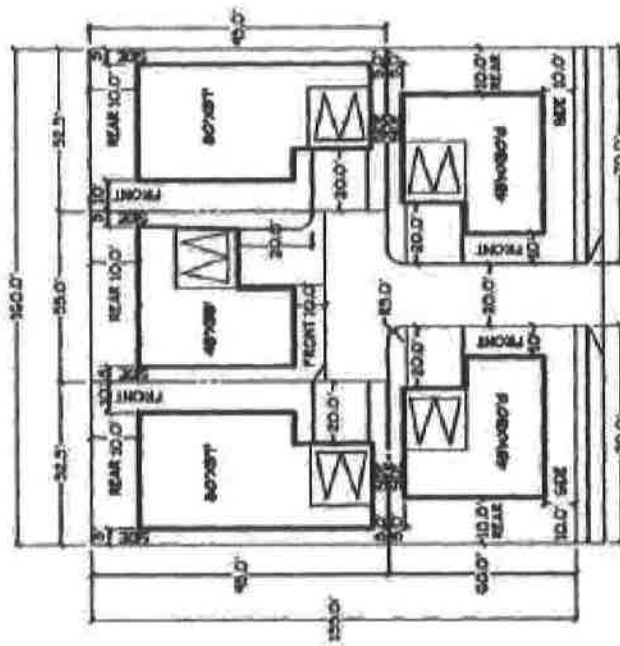


TYPICAL LAYOUT - 3-PAK & 5-PAK
CLUSTER HOUSING

FIGURE III-3A



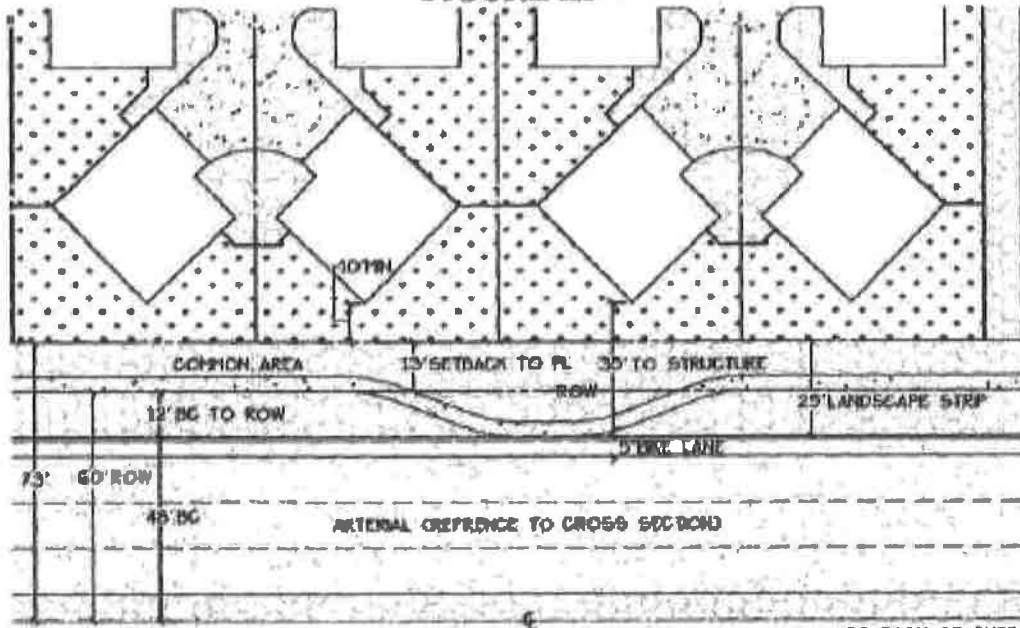
ALTERNATING 3-PAK
 NOTE: ALL ENTRANCES
 SHOULD BE REFERRED
 TO AS ENTRANCE



5 - PAK
 NOTE: ALL ENTRANCES
 SHOULD BE REFERRED
 TO AS ENTRANCE

TYPICAL LAYOUT AND DIMENSIONING

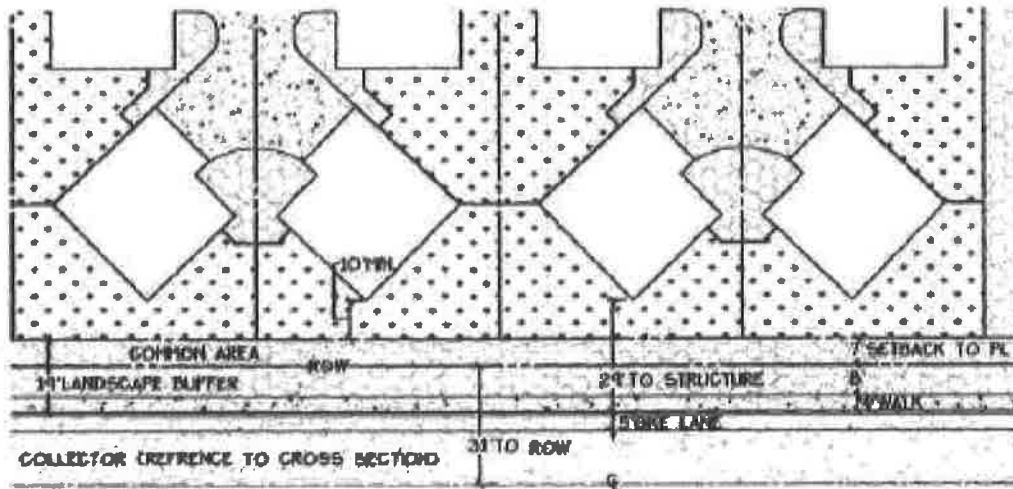
FIGURE III-4



BC-BACK OF CURB
 ROW-RIGHT OF WAY
 PL-PROPERTY LINE

SETBACKS RELATIVE TO ARTERIAL

35' TO STRUCTURE WITH A 25' LANDSCAPE BUFFER FROM BC TO PL

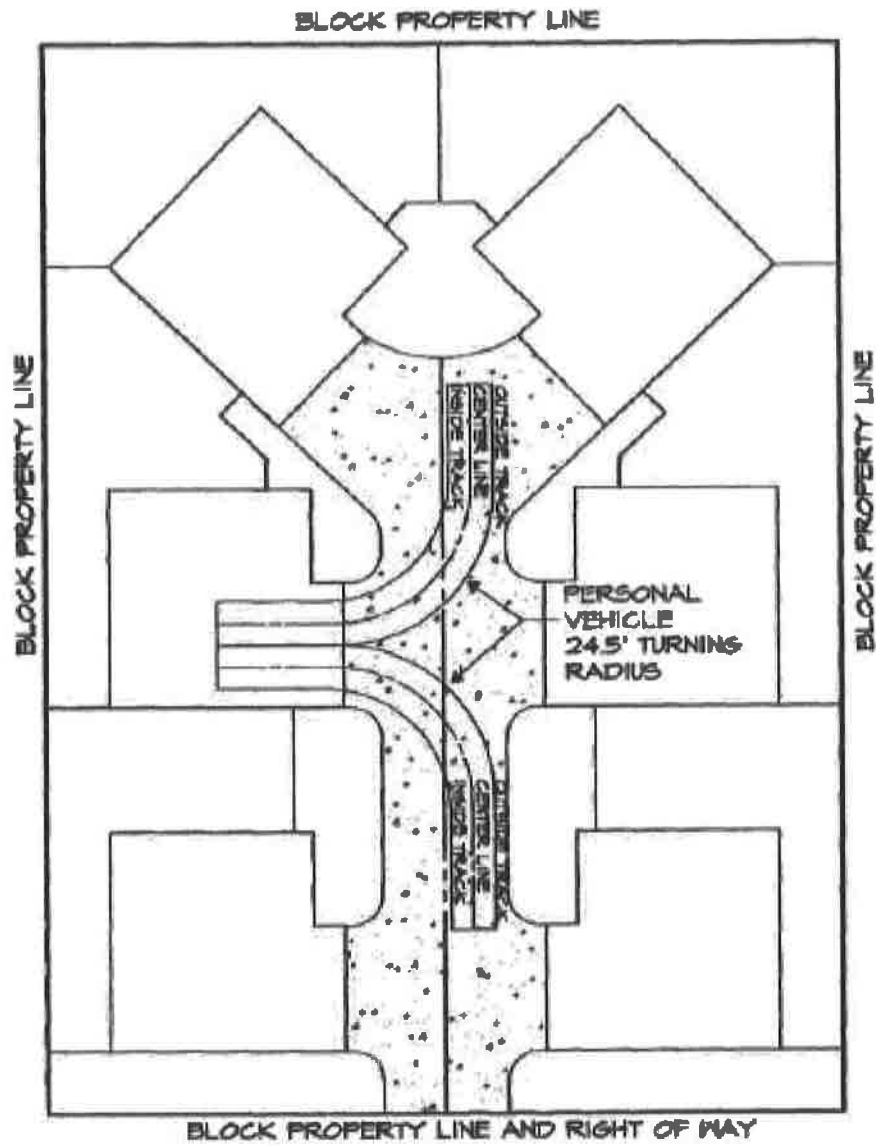


ROW-RIGHT OF WAY
 PL-PROPERTY LINE

SETBACKS RELATIVE TO COLLECTOR

29' TO STRUCTURE WITH A 19' LANDSCAPE BUFFER FROM BC TO PL

FIGURE III-5



TURNING MOVEMENTS

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MF15

Description

MF15 (multi-family) for *Pioneer Meadows* is located in two (2) areas: a thirty (30) acre parcel located along Vista Boulevard and a twenty (20) acre parcel located in the southwest portion of the site.

The thirty (30) acre parcel located along Vista Boulevard contains densities of fifteen (15) dwelling units per acre (du/ac), yielding a maximum of 450 units. The main access into this parcel is common with the commercial site to the east. No primary access will be permitted on to Vista Boulevard. This site is bordered by Common Area on the west and north, Vista Boulevard on the south and Hills Drive on the east. The southern boundary, which is common to Vista Boulevard, will require a six foot (6') decorative masonry wall to reduce the noise generated from traffic along Vista Boulevard. All fencing standards shall comply with **Section IV - Page IV-14**.

The twenty (20) acre site is located in the southwest portion of *Pioneer Meadows* and contains densities of fifteen (15) dwelling units per acre (du/ac), yielding 300 units. The primary access serving this parcel will be located on the arterials running along the northern and eastern boundaries (Hills Drive). This parcel is bordered by an arterial on the northern and eastern sides, Common Area on the southern and western sides and a residential collector on the southeastern side (Rolling Meadows Drive). All fencing standards shall comply with **Section IV - Page IV-14**.

Each parcel or unit will provide recreational facilities for resident's use. These facilities will provide a minimum of three (3) activities including, but not limited to, club houses, swimming pools, multi-purpose courts for basketball, volleyball or tennis, common barbecue areas and tot-lot facilities. These facilities will be integrated throughout the multi-family area. If this multi-family area is developed in phases, each phase will be reviewed for sufficient recreational amenities during the **entitlement process**.

The cumulative density of all the MF15 areas must equal 10 du/acre or greater.

Parking

- One (1) per efficiency, studio, or one (1) bedroom.
- Two (2) per unit for two (2) bedroom or larger units.
- One (1) per twenty-five (25) spaces or a fraction thereof will be accessible spaces (refer to current City of Sparks' Standards or the Americans with Disabilities Act requirements, whichever is more restrictive).

Characteristics

- Maximum density - Fifteen (15) du/ac.

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- Maximum impervious coverage - Eighty percent (80%).
- Minimum landscaped area - Twenty percent (20%).

Lot Setbacks

- Front – Contiguous to Parking or Private Drive – 10 feet (10');
Contiguous to Local Street – Twenty feet (20');
Contiguous to Arterials - Thirty feet (30').
- Interior Side Yard – Ten feet (10')
- Rear - Ten feet (10').
- Exterior side yards - Twenty feet (20').
- Minimum building separation – Twenty feet (20')

Building Standards

- Ten feet (10') from back face of curb, parking or access roads.
- Any one-story building, a minimum of fifteen feet (15') between adjacent buildings; any two-story building, a minimum of twenty feet (20') between adjacent buildings; any three-story building, a minimum of twenty-five feet (25') between adjacent buildings. If a combination of building heights occurs, the taller building setback shall prevail.
- Maximum building height: Forty-five feet (45').
- Attached multi-family requires a minimum of 20% landscape area. Half (50%) of the landscape area may include: public areas (plazas, recreational amenities, etc.), utilizing decorative hardscape materials (pavers, stamped and colored concrete, etc.)

Parcel Size

- Minimum lot size - 15,000 square feet (SF).

Permitted Uses

- Sales office.
- Sales Trailer
- Condominiums and associated maps
- Accessory Buildings.

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- Temporary construction yards.

Uses Requiring a Site Plan Review

- Multi-family residential and apartment development under 110 units not requiring a tentative map.
- One single family dwelling of a permanent nature for a Caretaker or Manager.

Uses Requiring a Special Use Permit

- Buildings in excess of forty-five feet (45') in height.
- Multi- family residential and apartment development of 110 units or more not requiring a tentative map.
- Childcare facilities.
- Above ground public utility structures

MF15A

Description

MF15A (multi-family) for *Pioneer Meadows* is located in two (2) areas: a seventeen (17) acre parcel and a twenty-four (24) acre parcel located in the central portion of the site.

The seventeen (17) acre site is located in the central portion of *Pioneer Meadows* and contains densities of fifteen (15) dwelling units per acre (du/ac), yielding a maximum of 255 units. The primary access is off the residential collectors that surround the site. Alternative access servicing this parcel could be located at the east boundary. All fencing standards shall comply with **Section IV - Page IV-14**.

The twenty-four (24) acre site is located in the central portion of *Pioneer Meadows* and contains densities of fifteen (15) dwelling units per acre (du/ac), yielding 360 units. The primary access is off the residential collectors that surround the site. All fencing standards shall comply with **Section IV - Page IV-14**.

The cumulative density of all the MF15A areas must equal eight (8) du/acre or greater.

Permitted Uses

- Single family residential (reference SFR6 sections).
- Sales office from a trailer.
- Multi-family residential.
- Single family residential – cluster housing (reference SFR10 section)
- Neo traditional residential (reference NT section)
- Gated communities with private streets.

Uses Requiring a Site Plan Review

- Multi-family residential and apartment developments under 110 units not requiring a tentative map.

Uses Requiring a Special Use Permit

- Multi-family residential and apartment developments of 110 units or more not requiring a tentative map.
- Above ground public facility structures.

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Development Criteria

- Minimum development size – Ten (10) acres.
- Minimum lot frontage – Reference individual classifications.
- Side yard setback – Reference individual classifications
- Front yard setback – Reference individual classifications
- Rear yard setback – Reference individual classifications
- Maximum building height – Reference individual classifications
- Rear yard abutting main arterial – Reference individual classifications
- Rear yard abutting residential collectors - Reference individual classifications
- Attached multi-family requires a minimum of 20% landscape area. Half (50%) of the landscape area may include: public areas (plazas, recreational amenities, etc.), utilizing decorative hardscape materials (pavers, stamped and colored concrete, etc.)

NT

Description

NT (Neo-Traditional) is an acceptable use within the residential classifications of *Pioneer Meadows*, as defined within this Handbook. NT is an alternative to typical residential developments. The NT classification can incorporate the following criteria into the development standards:

- Reduction of public street sections.
- Alleyways.
- Reduction of setbacks.
- Relocation of driveways to the rear of the unit, accessible via the alleyways.
- Gated communities with private streets.
- Common area features such as village greens, paseo walkways and parkway landscaping.

Permitted Uses

- Single family residential.
- Multi-family residential.
- Temporary and permanent model home complexes and sales offices.
- Temporary construction yards.
- Gated communities with private streets. *See Figure III-6 "Alternative Private Street section".*

Special Use Permit

- Above ground public utility structures.

General Criteria

Overall densities for NT developments shall not exceed densities set forth in this handbook.

- All front yards and corner side yards shall be landscaped by the builders and maintained by a Homeowner's or Landscape Maintenance Association.

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Section III-Development Standards and Descriptions

- Where provided, sidewalks and pathways within the development shall be Portland Cement Concrete a minimum of four (4) feet in width.
- Where provided, common area paseos shall include pathways and landscaping.

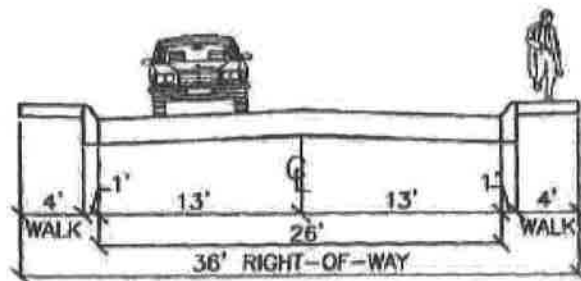
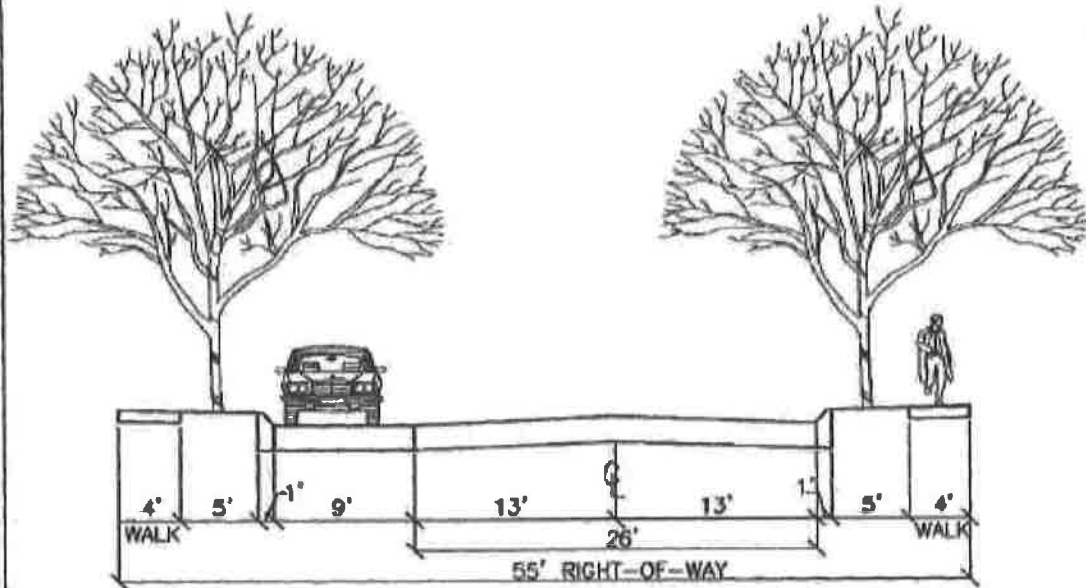
Lot Design Criteria

- Minimum lot size – 2,500 SF.
- Minimum lot frontage – 25' feet.
- Minimum Front yard setback – 10 feet to structure; 20 feet to garage; 5 feet to covered or uncovered porch.
- Minimum Side yard setback – 3.5 feet (interior lots); 3.5 feet (corner lots).
- Minimum building-to-building separation – 7 feet (even to eave separation 6 feet).
- Minimum Rear yard setback – 10 feet (standard lots); 3.5 feet (Z lots). *See Figure III-7 – 'Z-Lot'.*
- Minimum usable rear yard – 300 sq. ft.
- Maximum building height – 30 feet.

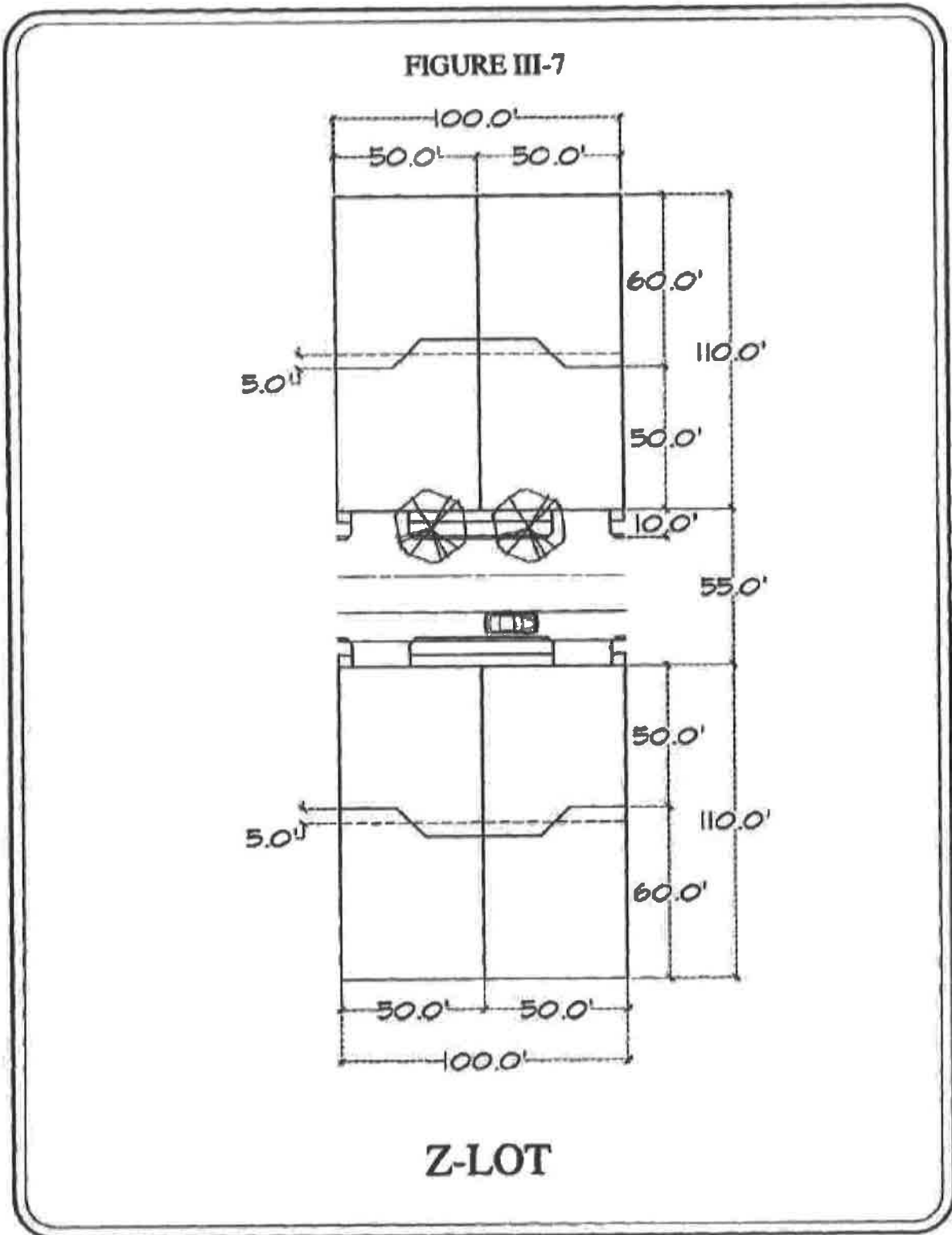
Development Criteria

- Minimum street sections – 26 feet flowline-to-flowline (*See Figure #1 'Alternative Private street section'*).
- Minimum sidewalk requirements – 4 feet on both sides of the street.
- Minimum parkway area, if provided – 5 feet.
- Minimum on street parking requirement – one side of the street (9 feet wide) or equivalent provided in parking bays.
- Minimum on site parking requirement – one space per bedroom (tandem garages or driveway spaces permitted).
- Minimum width of paseo connections – 10 feet (paseo's are not required).

FIGURE III-6



ALTERNATIVE PRIVATE STREET EXAMPLES



GENERAL COMMERCIAL (GC)

Description

Pioneer Meadows includes one parcel set aside as General Commercial (GC), totaling twenty-five (25) acres.

The parcel is located in the southeast corner of *Pioneer Meadows* where it abuts Vista Boulevard and is split between Wingfield Hills Drive. The primary access for the western parcel is common to the 30 acre MF15 parcel to the west. The access for the eastern parcel is common to the 14 acre BP-A parcel to the north via a roundabout. Ingress right and egress right turning movements and uncontrolled left turning movement into the site (reference to the traffic report included in *The Supplement to the Development Standards Handbook for Pioneer Meadows*) will be the sole turning movements allowed on to and off of Vista Boulevard and Wingfield Hills Drive. A twenty-five foot (25') landscape buffer will be required along both of the arterials that border the site (reference to **Section VII-Infrastructure and Public Services-Figure VII-10**).

Permitted Uses

Once the building has been constructed, the uses listed below as ***Uses Requiring a Site Plan Review*** shall be permitted without additional review. If the site plan and/or exterior of the building changes in any way, then it shall no longer be considered a permitted use and shall require additional ***Site Plan Review***.

Uses Requiring a Site Plan Review

- Retail stores and personal service shops conducted wholly within a building.
- Shopping facilities.
- Comparison goods.
- Retail and wholesale stores.
- Business Park (not to exceed ten (10) acres in any one (1) designation area).
- Recreation facilities and fitness centers.
- Permanent sales offices and association headquarters.
- Temporary sales offices and association headquarters.
- Public facilities.
- Clinics and Medical Offices.

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- Temporary construction yards for site construction.
- Offices (business, professional, real estate, banking, etc.)
- Restaurants.
- Child care facilities requiring more than one caregiver.
- Assisted living care facilities.
- Convenience stores.

Uses Requiring a Special Use Permit

- Amusement and entertainment centers.
- Bars, brew-pubs and taverns.
- Churches, clubs, lodges or social halls.
- Drive through facilities.
- Outdoor sales and service operations.
- Automotive service stations.
- Gas stations.
- Outdoor storage of material related to nursery, garden and building materials. This outdoor storage shall be accessory to retail stores.
- Structures over forty feet (40') in height.

Characteristics

- Maximum impervious coverage - Eighty-five percent (85%).
- Minimum landscape area - Fifteen percent (15%).

Lot Setbacks

- Front and exterior side - Twenty feet (20').
- Side or rear yard abutting any residentially zoned property - One foot (1') for each one foot (1') of building height.
- Side - Zero feet (0').

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- Rear - Ten feet (10').
- Maximum building height - Forty feet (40').

Parking Standards

Commercial centers with mixed uses will be encouraged to use shared parking. Shared parking will reduce the “seas” of asphalt that remain unused much of the time, reduce runoff and maintenance costs and increase the aesthetic value of the center.

- Parking to comply with the City of Sparks’ Municipal Code.
- Commercial centers greater than ten (10) acres in size with mixed uses will be encouraged to use shared parking as outlined in the City of Sparks’ Municipal Code.

Lot Design Criteria

- Minimum lot size - 6,000 SF (interior), 7,000 SF (exterior).
- Minimum corner lot size - 7,000 SF.
- Minimum lot width - Sixty feet (60') (interior), seventy feet (70') (corner).
- Maximum coverage of lot by structure - Fifty percent (50%).

BUSINESS PARK (BP)

Description

In contrast to other Commercial District options, which establish suitable locations for a wide range of activity, it is also desirable to provide some locations for individual corporate facilities. These types of facilities offer a high level of environmental standards in a campus like setting and can bear a symbiotic relationship with nearby residential districts. Benefits from such an association may include: an interrelation of support services and activities, a broader available pool of employment opportunities for nearby residents and minimization of travel for employees within nearby residential areas.

BP must bear a special burden of compatibility with adjacent uses. As such, use and site standards must be designed to allow this district to function in areas adjacent to residential areas and areas of other uses. BP permits research and development, accessory assembly, corporate facilities, offices, financial and medical facilities, along with retail services selected to support these broader activities.

Two sites exist within the Pioneer Meadows with the Business Park (BP) designations. An eighty-eight acre (88 ac.) site in the northwest corner (BP), and a fourteen acre (14 ac.) site in the southeast corner adjacent to ~~Vista Blvd~~Wingfield Hills Drive (BP-A).

Permitted Uses

Any new buildings, or building additional shall require a site plan review.

Uses Requiring a Site Plan Review

Within the BP areas, no building, structure or use shall be made of land for any purpose other than any one of the following, provided that such use shall meet the standards as set forth in this section:

- Medical offices and clinics.
- General offices.
- Research and development, for corporate or regional headquarters.
- Medical and dental laboratories.
- Warehousing, ancillary to research and development up to fifteen percent (15%) of the total floor area.
- Assembly of finished products or sub-assemblies, so long as the primary use of the property is not the basic processing and compounding of raw material or food products.

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- Assisted living care facilities, and Skilled Nursing Facilities are allowed in the BP area (88-acre site in the northwest corner) including individual Units with or without kitchens, duplex residential units, and others uses commonly associated with a Assisted Living/Skilled Nursing Facility and only when accessory to a medical hospital.
- Assisted living care facilities, and Skilled Nursing Facilities are allowed in the BP-A (14-acres southeast site) including Individual Units with or without kitchens, duplex residential units, and others uses commonly associated with a Assisted Living/Skilled Nursing Facility.
- Conference/Convention Center
- Pharmaceutical Manufacturing
- Software Development
- Broadcast Studios
- Governmental Institutions, including but not limited to, Public Safety Facilities and other City offices
- Hospitals
- Veterinary offices; No outdoor kenneling allowed.
- Hotels specifically oriented to business travel.
- Building and loan associations, brokerage houses, saving and loan associations, finance companies, title insurance and trust companies.
- Facilities for the furnishing of meals and sales of refreshments and personal convenience items solely to the employees of such establishments provided that such facilities be within a building or enclose such that there are no external signs or other evidence of such use. There shall be no external access to such a use.
- Parking structures.

Ancillary uses at a maximum of 15% of the cumulative business park area that has received Site Plan Review approval

- Public facilities.
- Restaurants, subject to the following conditions:
 1. All alcoholic beverages shall be sold only for consumption on the premises.
 2. Outdoor dining permitted as an accessory use.

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3. No restaurant shall be located closer than forty feet (40') from a residential district.

- Recreational facilities and/or health spas.
- Pharmacy, provided that such facility be within a building or enclosure such that there are no external signs or other evidence of such use. There shall be no external access to such a use.
- Support services, including but not limited to: Banks, Fitness Centers, Convenient Stores, Daycare Centers, Copying Services, Barber Shops, Beauty Salons, and Restaurants.

Characteristics

- Minimum lot frontage - Fifty feet (50').
- Maximum impervious area - Eighty percent (80%).
- Minimum landscaped area – Twenty percent (20%).
- Minimum lot width - One hundred feet (100').
- Minimum lot size - 20,000 square feet (SF).
- Minimum distance between buildings - Thirty feet (30').
- Cumulative Minimum Floor Area Ratio (F.A.R.) – 0.50
Cumulative means the Business Park (BP designation only, not included BP-A) as a whole – 88 acres.

Lot Setbacks

- Front and exterior side – Fifteen feet (15') to structure*, twenty (20') to parking areas. Encourage parking on sides and rear.
- Side and rear - Twenty feet (20') to structure*, five feet (5') to parking.
- Minimum Building Separation – Twenty feet (20')
- Minimum Parking Setback Along Residential – Ten feet (10')

*Minimum setbacks along a residential district to increase when building height exceeds thirty feet (30') in height - one foot (1') increase for every one foot (1') in height.

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Parking

Business Parks with mixed uses will be encouraged to use shared parking. Shared parking will reduce the “seas” of asphalt that remain unused much of the time, reduce runoff and maintenance costs and increase the aesthetic value of the center.

- Parking to comply with the City of Sparks’ Municipal Code.
- Business Parks greater than ten (10) acres in size with mixed uses will be encouraged to use shared parking as outlined in the City of Sparks’ Municipal Code.
- Other than required access drives, no parking or loading shall be located in the required setback adjacent to public streets or to residential districts.

Fencing

- A six foot (6’) decorative masonry wall shall be provided on any lot line which abuts a residential district, unless that lot line abuts a street. All walls to be treated with an anti-graffiti coating.
- Any loading space or dock shall be screened by an eight foot (8’) high or higher solid, decorative wall when located adjacent to a public street or residential district as determined under the ***Site Plan Review***. All walls to be treated with an anti-graffiti coating.

PARK

Description

Pioneer Meadows has several parks located throughout the site. The parks generally are accessible by a trail system that surrounds the development. ~~The park located contiguous to the school site is approximately four and one half (4.5) acres in size. This park is located in the northern portion of the site, encompassed by SFR4, and is accessible by bike lanes and walkways associated with the residential collector.~~ The two and one half (2.5) acre park located in the eastern portion of the site is also encompassed by SFR4, and accessible by both the trail system and residential collectors associated with the contiguous Villages. A second~~The third~~ park, approximately 5 acres in size, is a three (3) acre park located along the western project boundary adjacent to the MF-15 within the large Common Area south of Wingfield Hills Drive, at the terminus of a residential collector bounded by MF15, MF15A, RD10A and Common Area. It is accessible by bike lanes along with trails and walkways associated with the residential collectors and the trail system.

The parks described within this development are designed for pedestrian use by the property owners within the surrounding neighborhoods. All park designs within this development will be reviewed and approved by the City of Sparks' Parks and Recreation Director and maintained by the City of Sparks. The area and location of the park sites within Pioneer Meadows may be revised with the administrative approval of both the Parks and Recreation Director and the Administrator of the Community Development Department. A Master Plan Amendment will not be required to relocate the park sites, however the sum total acreage of the parks within Pioneer Meadows must equal 9.07.5. A relocated park site may only be replaced with a residential land use designation and a park site may only be relocated to an area with a residential land use designation or Common Area. If a change in land use acreage results in an increase of residential density from the approved Maser Plan, then a Master Plan Amendment may be required as determined by the administrator.

~~———— The four and one half (4.5) acre park located contiguous to the school site may be incorporated with the school. This may allow for the design of multi-purpose fields, such as baseball or soccer fields. If this is the case, field lighting will be acceptable within this planned development, with approval of the Administrator.~~

Parcel Characteristics

- ~~• Maximum five (5) acres.~~
- Setbacks for structure/fields/play equipment - Ten feet (10')
- Maximum structure height - Twenty-five feet (25').

Special Considerations

The park may include:

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Section III-Development Standards and Descriptions

- Half court basketball courts.
- Soccer fields
- Playground area
- Parking

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COMMON AREA

Description

Pioneer Meadows contains 146.7 acres of Common Area. The City of Sparks requires a minimum of twenty percent (20%), or 128 acres. The majority of Common Area associated with *Pioneer Meadows* is located around the perimeter of the site, with three (3) plus miles of trails connecting all residential areas, the parks and the regional park located at the northwest border of the site.

A regional drainage facility is located within the Common Area of this development. This facility is to be maintained in part by the Homeowners' Association and the City of Sparks. This drainage facility is to be redesigned with a twenty foot (20') cut off channel on each side of the bottom width to channelize nuisance flows and reduce sediment deposition throughout the channel. The bottom width and banks of the channel will be seeded with native vegetation (no trees) to the approval of the Administrator. The areas which are associated with the trail system will be planted with trees, intermittently spaced in groups, and singles to the approval of the Administrator. (reference to **Section VII-Infrastructure and Public Services-Figure VII-8**).

The Homeowners' Association (HOA) will maintain the following areas: all areas depicted as Common Area on all plat maps (excluding private common areas within the Multi-Family, Commercial and Business Park boundaries); all landscaped areas along and within the dedicated right-of-ways, as described within this Handbook; all private drainage facilities located within the designated Common Areas (excluding onsite drainage facilities within the Multi-Family, Commercial and Business Park boundaries); all infrastructure and associated areas relating to the trail systems designated within this Handbook. Each developed parcel will clearly indicate all privately (HOA) or publicly maintained facilities to the satisfaction of the Administrator and the Development Review Committee (DRC) (reference to **Section IV-Design Standards** for definition of DRC-**Page IV-2**).

The regional drainage facility will be maintained both by the City of Sparks and the HOA as previously stated. The nuisance channels within the drainage facility will be maintained by the City of Sparks. The remaining portions of the facility will be maintained by the HOA at the direction of the Public Works Director (reference **Figure VII-8**).

Additional improvements within the Common Area will be allowed. If the HOA decides to add additional improvements into the Common Area not described within this Handbook, the appropriate permits and approvals from the City of Sparks will need to be obtained. Approval by the Development Review Committee will also be required.

If the City of Sparks wishes to add additional recreational facilities in the designated Common Areas not described within this Handbook, the appropriate permits and approvals will need to be obtained. All additional recreational improvements requested by the City of Sparks within the designated Common Area will be maintained and operated by the City of Sparks. Concurrently with the first development, the appropriate

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easements for improvements will be given to the City of Sparks at the direction of the Public Works Director.

If the Homeowners' Association, Developer or the City of Sparks constructs multi-purpose fields, such as baseball or soccer fields, within the Common Area, field lighting will be acceptable within these areas, with the approval of the Administrator. This language will be included in the Covenants, Conditions and Restrictions (CC&R's) and the Development Agreement.

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SCHOOL SITE

Description

Pioneer Meadows has designated approximately eight and half (8.5) acres for an Elementary School site. The area and location of the school site within Pioneer Meadows may be revised with the administrative approval of both the Washoe County School District and the Administrator of Community Development Department. A Master Plan Amendment will not be required to relocate the school sites. The following **Permitted Uses** and **Uses Requiring a Site Plan Review** define alternatives for the site if the Washoe County School District relocated the school site or no longer desires the site. Washoe County School District must purchase the site within five (5) years of the approval of this Development Standards Handbook (PCN06052) or the property will convert to residential use at a maximum density of four dwelling units/acre.

Permitted Uses

- Recreation areas.
- Public schools.
- Parks.
- Accessory uses.
- SFR4 (residential use compliant with SFR4 Zoning standards subject to release of the property from the Washoe County School District). The total number of dwelling units allowed with the Pioneer Meadows development is not to be exceeded.

Uses Requiring a Site Plan Review

- Private recreation clubs.
- Churches.
- Private schools.
- Structures over thirty feet (30') in height.
- Public buildings (and facilities).

The following standards pertain to all potential non-residential Uses on the site.

Landscaping

- Twenty-five foot (25') wide landscape area along all residential collectors.

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~~Lot Setbacks~~

- ~~• Twenty-five feet (25') along residential collectors.~~
- ~~• Rear and side yards abutting residential areas: fifty feet (50').~~

~~Building Standards~~

- ~~• Five feet (5') from back face of curb.~~
- ~~• Ten feet (10') between buildings.~~
- ~~• Thirty foot (30') maximum height.~~
- ~~• Parking to accommodate use and to be approved by the Administrator at plan review.~~

~~Lot Design Criteria~~

- ~~• Minimum lot size - One (1) acre.~~
- ~~• Minimum lot width - 120 feet.~~

~~Accessory Structures~~

~~Accessory Structures shall comply with the City of Sparks' Municipal Code.~~

SECTION IV
DESIGN STANDARDS

ARCHITECTURAL STANDARDS

Residential

All residential developments within *Pioneer Meadows* will comply with the following criteria:

- Each single family Village developed by a home builder shall have a minimum of four (4) models each having a minimum of three (3) elevations.
- Front yard setbacks shall vary (reference to **Figure III-1**).
- Flat or rock covered roofs will not be acceptable. All roofing material will be a minimum of thirty (30) year architectural class A grade composition asphalt shingles or concrete tile. Air conditioning units, heating units, etc. shall not be mounted upon or attached to any roof structure.
- Exterior wall surfaces shall be wood siding, stucco, brick, stone or a combination subject to the approval of the Administrator.
- Detailing and articulation shall be required on all elevations to the approval of the Administrator (front, side and rear) as well as on any accessory structure(s).
- The architectural review process for residential developments is as follows: All building elevations will be submitted to the Design Review Committee (DRC) (described below under Non-Residential). The DRC will review building elevations and material for conformance with the **Architectural Standards** set forth within this Handbook.

Non-Residential

All non-residential (including multi-family) developments within *Pioneer Meadows* will comply with following criteria:

- A Design Review Committee (DRC) will be set up to review plans for the development of all non-residential areas within *Pioneer Meadows*. The DRC will have the option to include one staff member from the City of Sparks and or one member of the City Council or Planning Commission. DRC responsibilities will include the review of site plans, building plans, elevations, materials, colors, fencing, signage, lighting, landscaping and parking arrangements for compliance with the architectural and design standards set forth within this Handbook.
- After approval from the DRC, all new non-residential (including multi-family) buildings and building additions shall require site plan review when a special use permit is not required.

The non-residential areas offer an opportunity to ensure a positive impact and reinforce the community character of *Pioneer Meadows* as well as the City of Sparks. The intent of these standards is to avoid long linear, square or rectangular buildings without architectural relief detail on the exterior facade. Pioneer Meadows should set standards

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for highly amenitized design and architecture. All non-residential areas within *Pioneer Meadows* will be subject to the following criteria:

- Long facades within public view shall be articulated by varying building mass, form and texture.
- Columns, posts, projections or exterior detailing shall be used to architecturally enhance the structures.
- Entry ways shall be delineated with architectural elements.
- Signage shall be incorporated into the overall design.
- A minimum of 3 ~~of~~ building materials shall be required.
- Commercial service areas and loading facilities shall be located in the rear or side of the structure screened from the view of the public right-of-way and shall be located away from any property line. Specific mitigation shall be provided when adjacent to residential uses.
- Roofs visible from the adjacent property or the right-of-way shall not appear flat. This shall be accomplished by the use of parapets or other architectural features incorporated into the structure.
- Air conditioning units, heating units and mechanical equipment mounted on the roof tops shall be screened from the view of the public.
- Any metal roof shall have a non-glare matte of flat finish.

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Section IV-Design Standards**

FIGURE IV-1



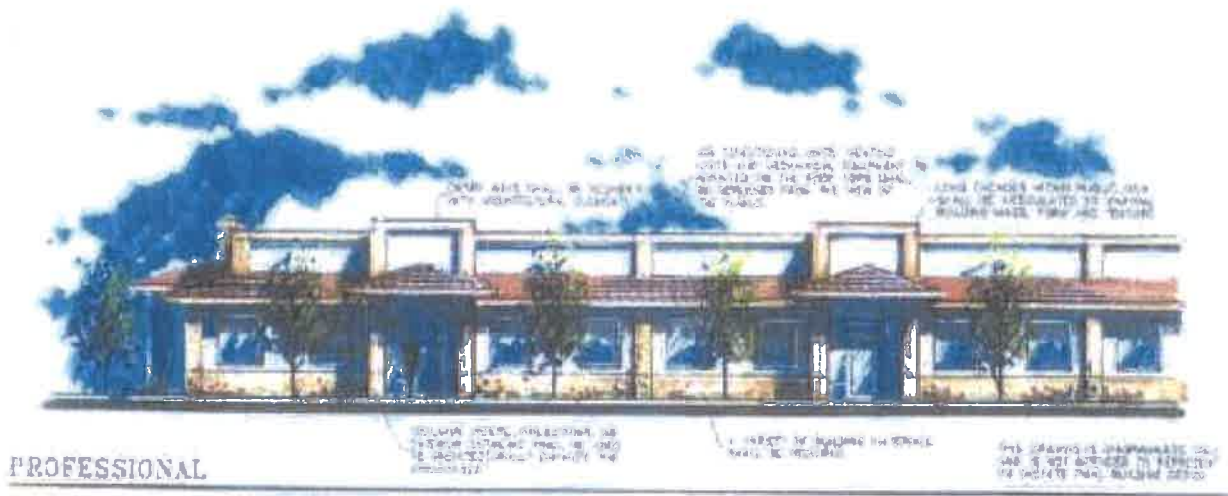
RESIDENTIAL



RESIDENTIAL

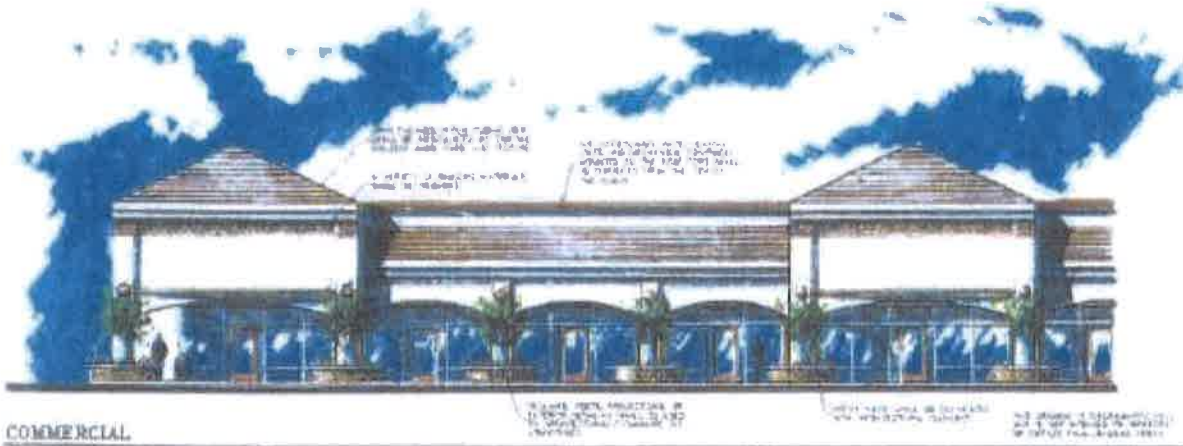
**Development Standards Handbook for Pioneer Meadows
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FIGURE IV-2



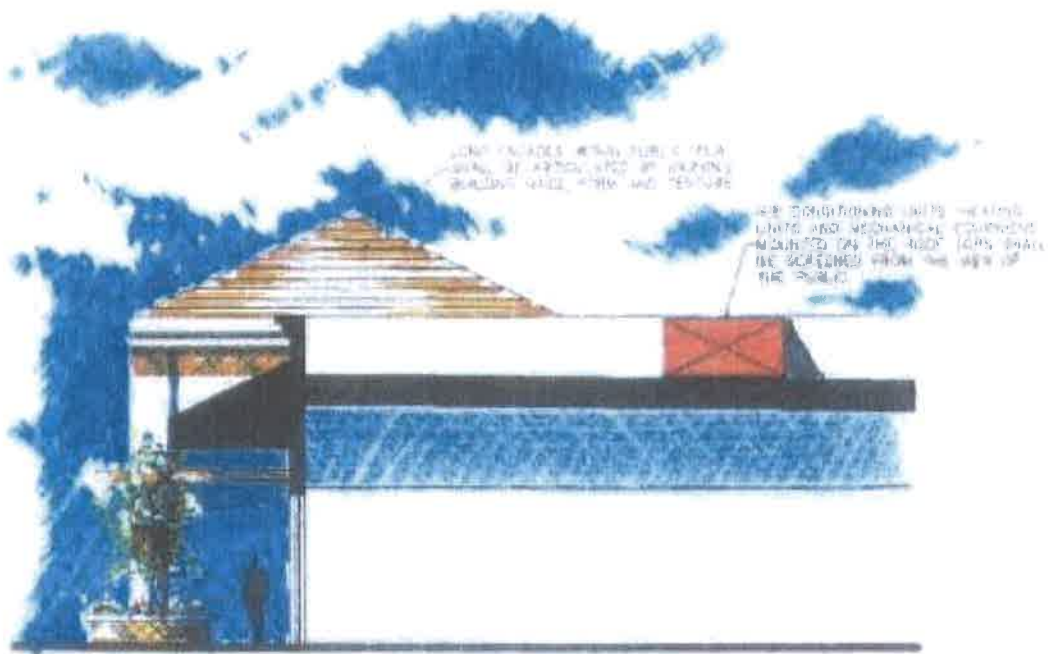
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FIGURE IV-3



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FIGURE IV-4



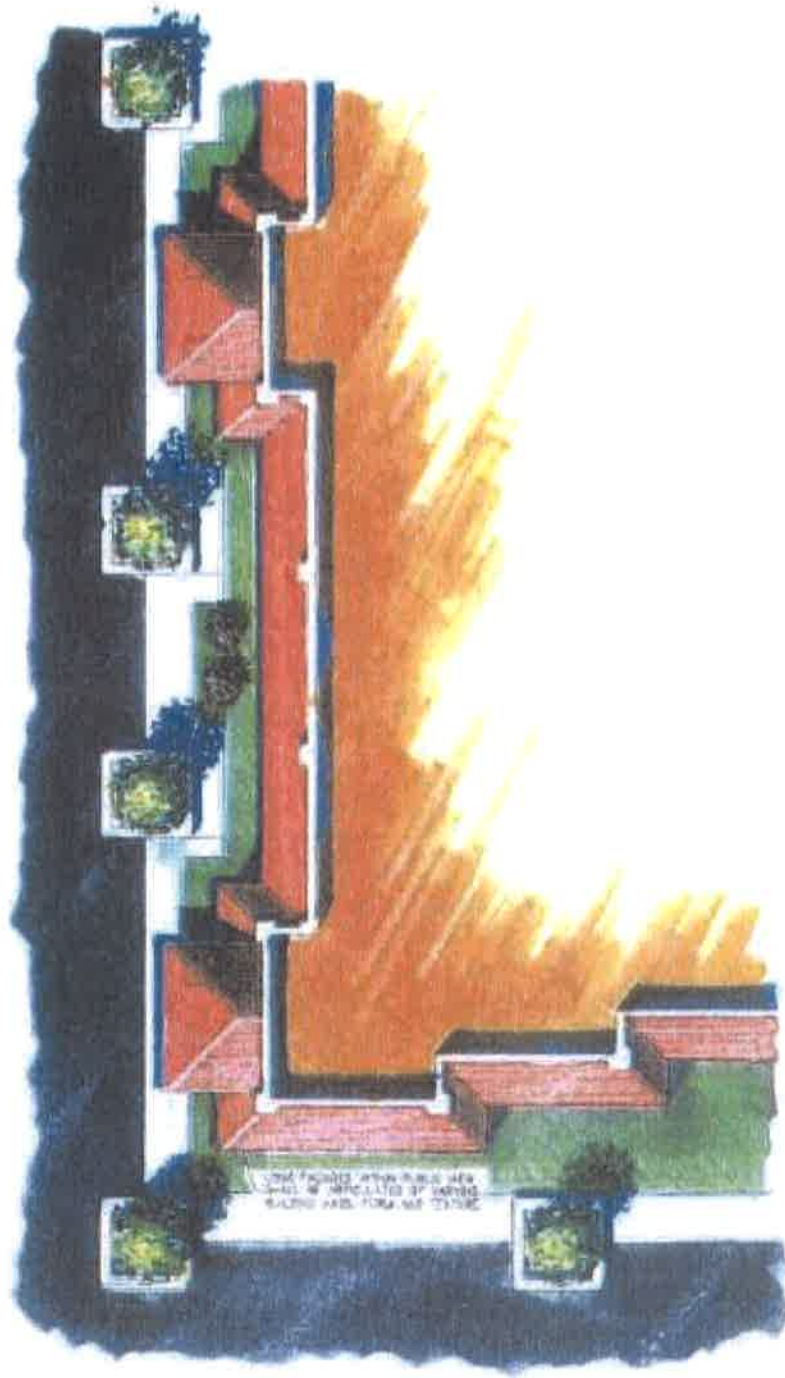
CROSS SECTION

THIS DRAWING IS FOR INFORMATION ONLY
AND IS NOT INTENDED TO SUPPORT
ANY DESIGN OR CONSTRUCTION
PROJECTS.

THIS DRAWING IS FOR INFORMATION ONLY
AND IS NOT INTENDED TO SUPPORT
ANY DESIGN OR CONSTRUCTION
PROJECTS.

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FIGURE IV-8



LOW PROFILE WITH PUBLIC USE
SHALL BE IMPLICATED BY CARVED
BUILDING AND FORMAL COURSE

PARTIAL PLAN

THE DRAWING IS UNAPPROVED FOR
USE IN ANY MANNER WITHOUT THE
WRITTEN PERMISSION OF THE ARCHITECT
OR ENGINEER.

LANDSCAPING

All classifications within *Pioneer Meadows* will be consistent with the City of Sparks' Municipal Code, Landscape Ordinance (except for area requirements, which are set forth within this Handbook).

Residential

All residential developments within *Pioneer Meadows* shall comply with the following criteria:

Single Family

The Builder of the individual units shall landscape the front yards in accordance with the following criteria:

- All front yards and exterior side yards shall require a fully automatic sprinkler system and drip irrigation system. The sprinkler system will be stubbed to the rear yard for future improvements to be completed by the individual homeowner.
- The Builder will landscape the front yards with exterior side yards with sod and incorporate one (1) tree with a 2" caliper for deciduous or 6' height for evergreen and three (3) shrubs (five (5) gallon size minimum) into this area.
- Landscape plans will be required at the time of submittal for all Common Areas and areas landscaped within the right-of-way for review and approval by the Administrator and the DRC. A typical front yard landscape plan shall also be reviewed and approved by the Administrator as part of the Final Map.

Multi-Family

- Landscape plans will be required at the time of Final Map or Special Use Permit submittal for all Common Areas (private and public) and areas landscaped within the right-of-way for review and approval by the Administrator and the DRC, prior to or at the time of adjacent development.

Non-Residential

All non-residential developments within *Pioneer Meadows* shall comply with the following additional standards:

- Where a lot line not on a street abuts a residential district, there shall be a landscape strip containing evergreens at least eight feet (8') in height planted twenty feet (20') on center along the property line.
- All non-residential developments will be required to submit landscape plans for each development to the DRC for review prior to submittal of the Site Plan Review or Special Use Permit.

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- Landscape plans will be required for all Common Areas (private and public) and areas landscaped within the right-of-way at the time of Site Plan Review or Special Use Permit.

Median Landscaping (Islands Within the Right-of-Way)

- Medians less than five feet (5') in width shall be concrete.
- Medians five feet (5') or greater in width and less than ten feet (10') in width shall be limited to low growth shrubs and ground cover (twenty-four inch (24") maximum height).
- Medians ten feet (10') or greater in width shall have a minimum two (2) to three (3) foot buffer (no grass) against the back face of curb.
- All landscaping within the medians and returns will be designed as to not impede or encroach into the visibility triangles.

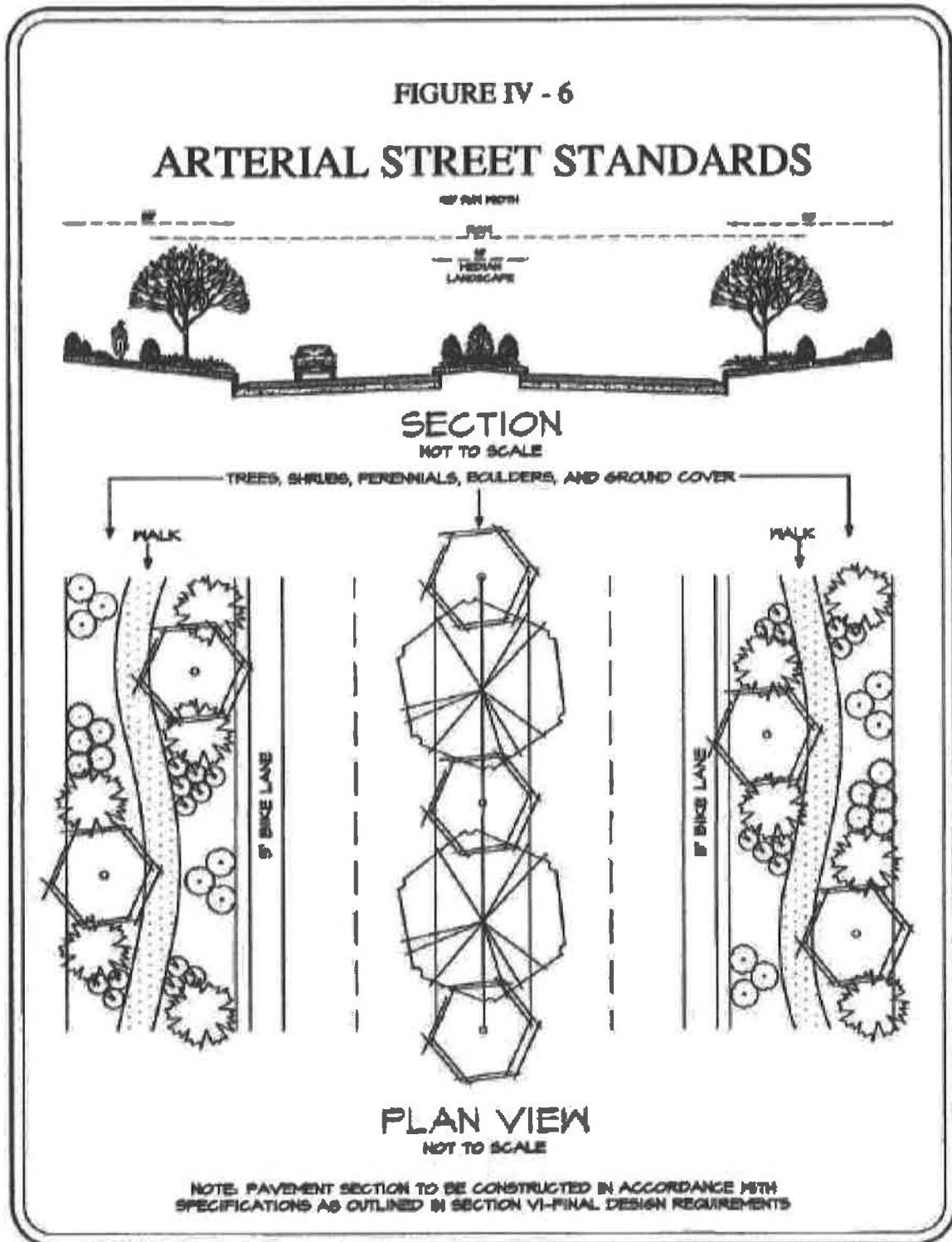
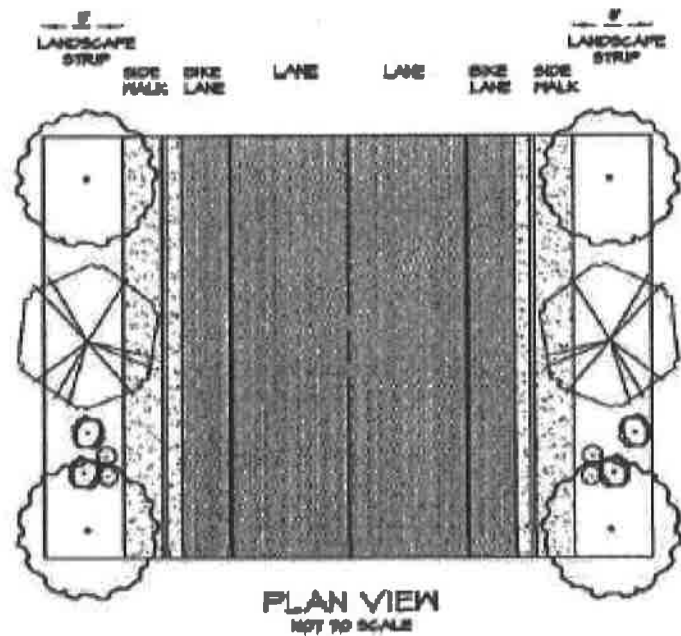
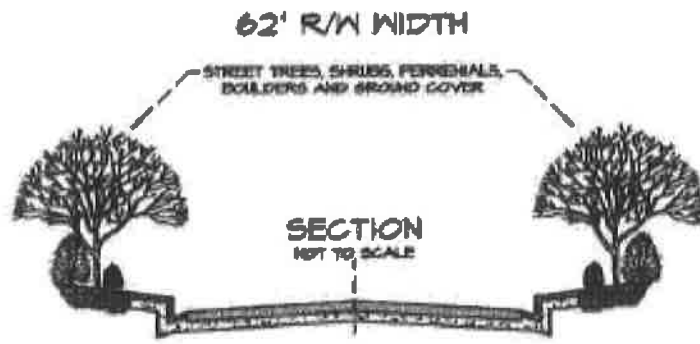


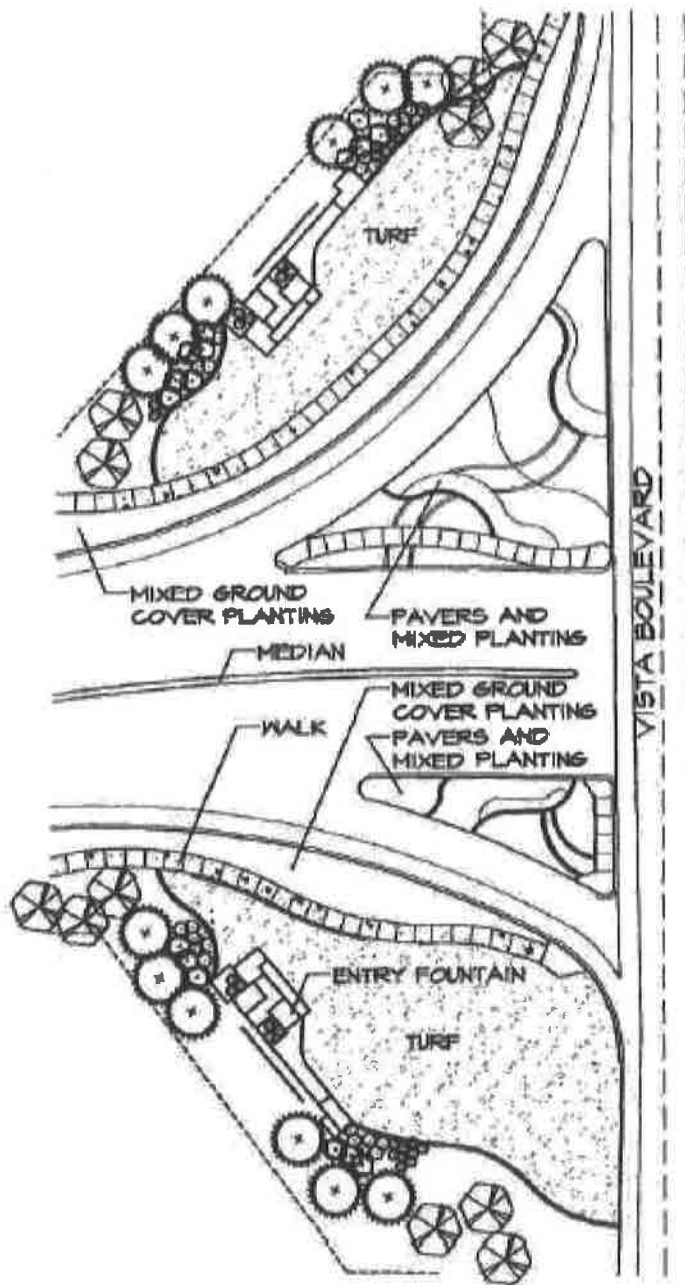
FIGURE IV - 7
COLLECTOR STREET STANDARDS



** NOTE:
PAVEMENT SECTION TO BE CONSTRUCTED IN ACCORDANCE WITH
SPECIFICATIONS AS OUTLINED IN SECTION VI-FINAL DESIGN REQUIREMENTS

** NOTE:
SIDEWALK ALONG NON-RESIDENTIAL ZONING CLASSIFICATIONS TO BE 8' IN WIDTH

FIGURE IV-8



ARTERIAL / VISTA BOULEVARD INTERSECTION

FENCING

Definitions

- Development Property Line - The property lines that describe future interior parcels located within the interior of *Pioneer Meadows*.
- Boundary Line - The property lines that make up the exterior boundary of *Pioneer Meadows*.

General

All developments within *Pioneer Meadows* will comply with the following criteria:

- Decorative perimeter fencing will be consistent throughout *Pioneer Meadows*.
- Decorative masonry block walls will be located between contiguous residential and non-residential areas (including multi-family) ~~(with the possible exception of the school-site)~~. All walls shall be treated with an anti-graffiti coating.
- Decorative masonry block walls will be located on the southeastern boundary of the non-residential areas (except in areas where non-residential abuts non-residential). All walls shall be treated with an anti-graffiti coating.
- Decorative masonry block walls will be located on the southern boundary line in the residential areas (MF15). All walls shall be treated with an anti-graffiti coating.
- Decorative perimeter fencing will be located on the perimeter of all residential areas. All walls shall be treated with an anti-graffiti coating.
- All decorative perimeter fencing will be stained or painted a like color throughout *Pioneer Meadows*. No homeowner will be allowed to alter the original stain or color of the decorative perimeter fencing. This criteria will be placed within the Covenants, Conditions and Restrictions (CC&R's).
- All trash enclosures will be constructed according to the standards set forth by the City of Sparks Design Standards Manual. The location of the trash enclosures shall be located to the approval of Sparks Sanitation and the Administrator.
- All rear and side yards located within residential areas (excluding MF15) will be fenced by the builder with a six foot (6') fence. This fencing will be consistent throughout each "Village", and consistent with the fencing standards.
- MF15 fencing will have several options:
 1. If fencing is placed abutting Common Areas, it shall be six foot (6') ornamental iron.

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2. If fencing is placed along the right-of-way, it shall be integrated into the landscaping and may be either decorative masonry or ornamental iron.
 3. Within the MF15 designation, a six foot (6') decorative masonry wall shall be required between multiple complexes and different land uses. All walls shall be treated with an anti-graffiti coating.
 4. All fencing will be subject to the review and approval of the DRC and the Administrator.
- ~~The fencing required around the school site will be determined by the Washoe County School District and the Administrator at the time of submittal of the school site.~~
 - The fencing around the park sites located within this development will be determined by the City of Sparks' Park Director at the time of submittal of the park site(s).
 - Reference to **Figures IV-9 through IV-11** for architectural depictions of fencing options.

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FIGURE IV-9

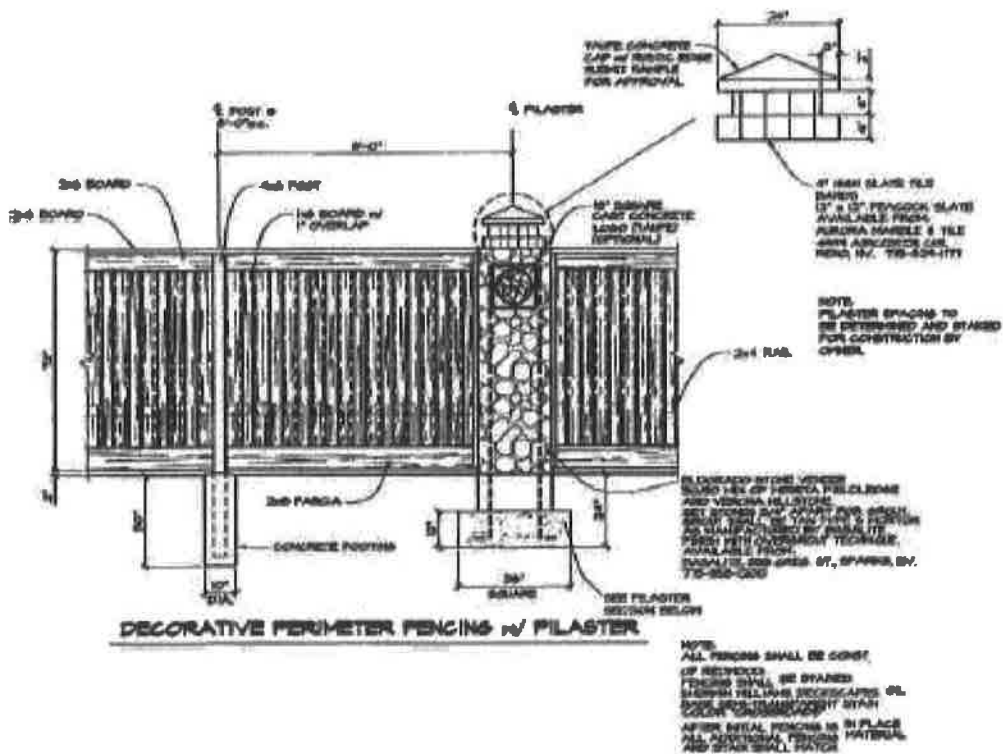
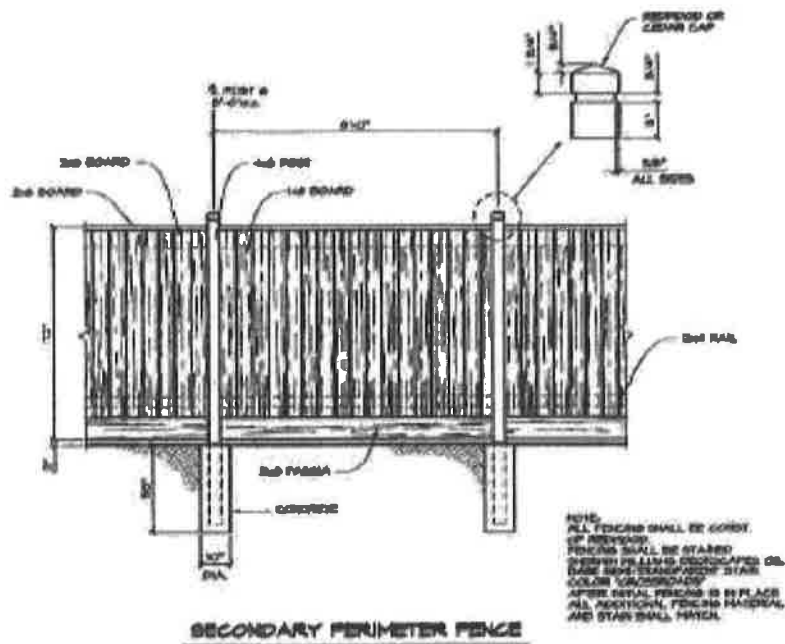
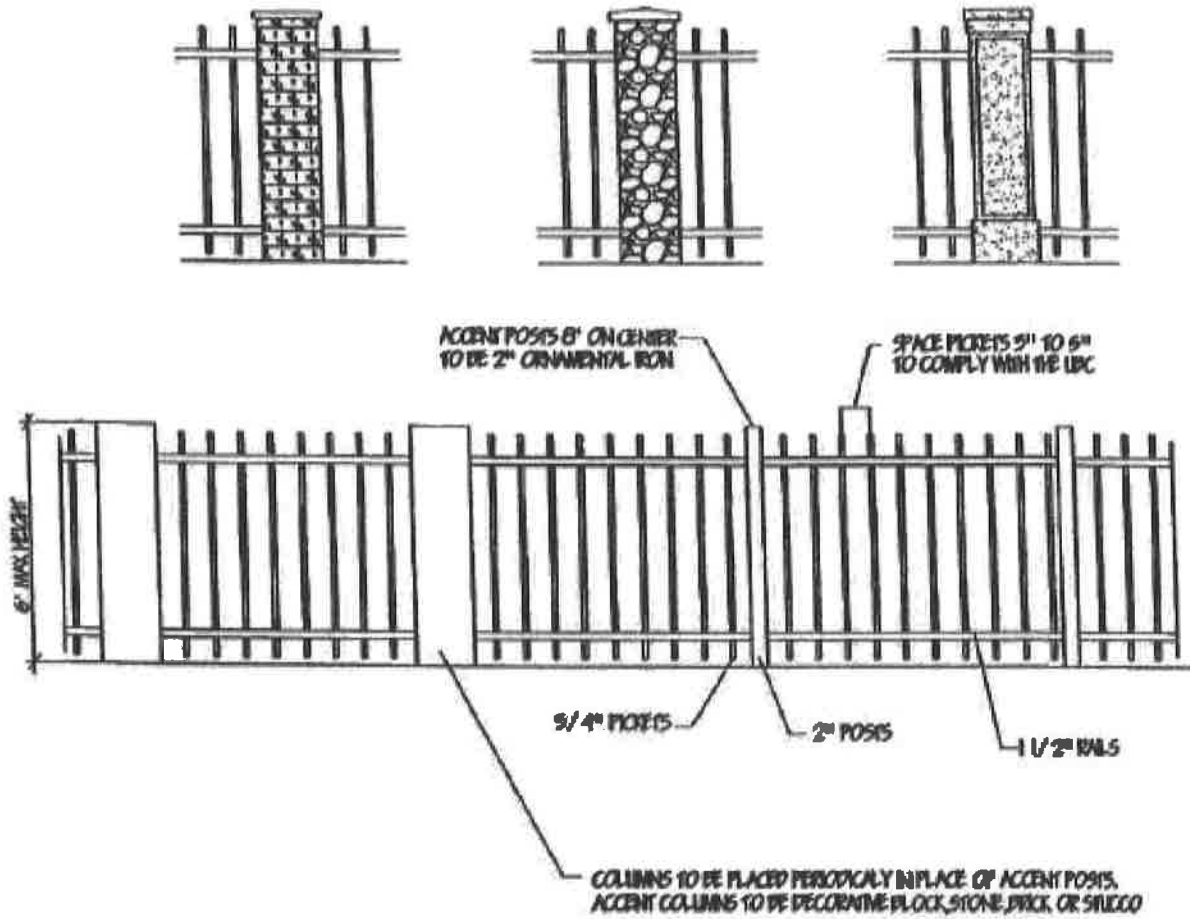
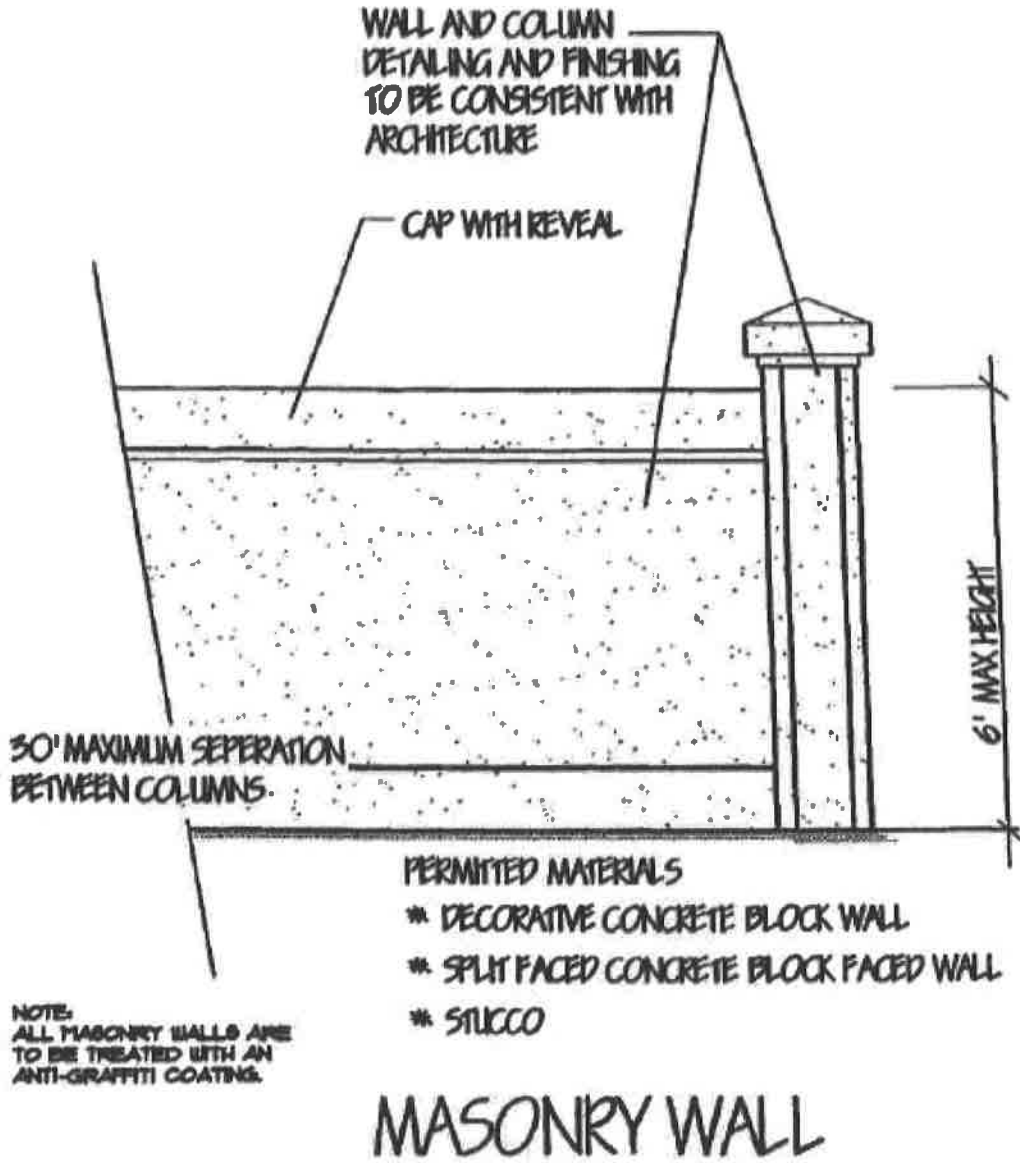


FIGURE IV-10



ORNAMENTAL IRON FENCING

FIGURE IV-11



SIGNS

General

Pioneer Meadows will incorporate directional signs and monument signs into the development. All signs within *Pioneer Meadows* will conform to the City of Sparks' Municipal Code with the following additions:

- General Commercial districts within *Pioneer Meadows* will comply with the current City of Sparks' Sign Ordinance set forth in the C2 - General Commercial District (ON-PREMISES SIGNS).
- Business Park districts within *Pioneer Meadows* will comply with the current City of Sparks' Sign Ordinance set forth in the PO - Professional Office District (ON-PREMISES SIGNS) with the following addition: all signs placed on the side of the building shall be visually integrated with the building architecture.
- All signs within this development will be approved by the DRC.
- A Signage Master Plan will be submitted with each non-residential development, detailing size, type, location, material and square footage and design style of the sign(s).
- No pole signs or canned plastic signs shall be permitted. Any free standing sign shall be decorative and integrate the area's architecture (pylon sign). Each site is limited to one (1) pylon sign.
- Engineered plans will be required for all pylon signs and monument signs, indicating that adequate sight distances are maintained. These plans shall be submitted with the application for a building permit for the sign.

Directional Signs

Directional signs will be located throughout *Pioneer Meadows* to direct the public to certain locations. The directional sign will be free standing and a maximum of seven feet (7') in height with individual panels designating the direction to certain developments within *Pioneer Meadows* (reference **Figure IV-13** for architectural depiction of directional signs).

Information placed on the directional signs will be limited to Commercial and Business Park Complexes (not individual businesses), residential villages, multi-family developments, schools and parks. Signs shall receive DRC approval before sign permit application to the City of Sparks. Signs may deviate from the attached diagram Figure 4-12 but must be approved by the DRC.

Development Standards Handbook for Pioneer Meadows
Section IV-Design Standards

Monument Signs

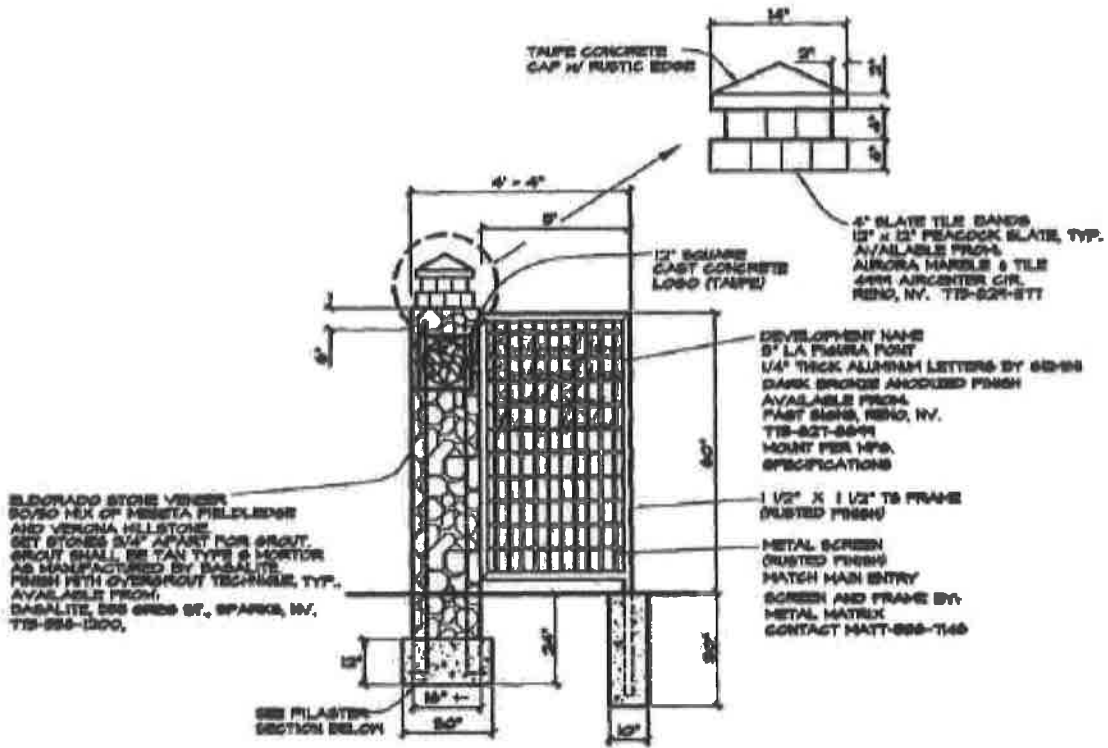
Monument signs are to be constructed at the main entrance to each individual development, both residential and non-residential uses (excluding some larger commercial developments). The monuments shall be low profile brick or stone encompassing a wood inscription, designating the name of that particular development or business. Signs shall receive DRC approval before sign permit application to the City of Sparks.

Main Entry Signs

Pioneer Meadows will allow Main Entry signs at the three (3) primary access points, Vista Boulevard and Hills Drive, and the western and northern boundaries of Kiley Drive. These signs shall be a low profile, monument type. The Entry sign shall go through the review process identified for non-residential architecture.

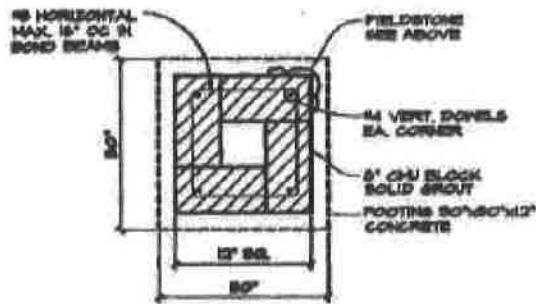
Development Standards Handbook for Pioneer Meadows
Section IV-Design Standards

FIGURE IV-12



LADDER DIRECTIONAL SIGN

N.T.S.



PILASTER SECTION

N.T.S.

TEMPORARY SALES OFFICE WITHIN A MODEL HOME AND MODEL HOME COMPLEX

Within *Pioneer Meadows*, the Builders will construct Model Home Complexes within their respective development(s). The following requirements will apply to all residential developments within *Pioneer Meadows*:

- The model homes, sales office and associated parking lot shall be landscaped to the approval of the Community Development Director.
- A building permit shall be obtained for the temporary sales office trailer. A landscaping and irrigation plan as well as a parking plan that meets the standards set forth in; **TEMPORARY SALES OFFICE WITHIN A MODEL HOME AND MODEL HOME COMPLEX**, shall be submitted for approval with the temporary sales office trailer. All landscaping, irrigation, and parking shall be installed prior to the issuance of a certificate of occupancy for the sales trailer.
- A permit for a temporary sales office trailer may be applied for once a final map has been approved for the subject village.
- A temporary sales office trailer may only be placed within the village its purpose serves.
- Once the construction of the model homes has been completed, the sales trailer shall be removed from the site and the area previously occupied by the sales trailer shall be landscaped or used for additional parking.
- The landscaping and irrigation shall be installed, per the approved plans, prior to the issuance of the Certificate of Occupancy.
- The landscaping plans shall exemplify xeriscape-style design and plant material on a minimum of one (1) model home.
- A paved parking area (minimum fifty foot (50') wide by ninety foot (90') deep) for the model homes and sales office will be constructed. The parking area shall include a minimum of five (5) standard and one (1) van accessible parking space that complies with City of Sparks' Standards at the time of development.
- Hours of operation shall be Sunday through Thursday, 9:00am to 7:00pm, Fridays and Saturdays 9:00am to 10:00pm.
- A Model Home Complex and sales office will be allowed for each Village. The sales office and models will be for that Village only. The sales office and models will be closed and converted to a conforming use subject to the approval of the Administrator.

CONSTRUCTION YARDS

Within *Pioneer Meadows*, construction yards will be necessary. The following requirements will apply to all construction yards within *Pioneer Meadows*:

- An all weather, dust free surface will be required for construction yards. Best Management Practices shall be used to protect the area from pollutants such as hydrocarbons.
- A six foot (6') fence with a solid gate will be constructed around the construction yard. All materials will be stored behind and below the fence, out of public view.
- Once the construction is completed within the particular development, the construction yard shall be removed. The area shall be reclaimed to the approval of the Administrator.
- The developer shall limit all construction and construction-related activities to the time between the hours of 7:00 a.m. to 7:00 p.m. Monday through Friday and 9:00 a.m. to 5:00 p.m. Saturday. An exception shall be allowed for large tenant concrete (10,000 sqft or larger) pours that shall be authorized by the City of Sparks prior to commencement of work. The developer shall install signs in both English and Spanish at all access point to the project that clearly indicate these limited hours of activity on-site prior to the start of any construction related activities to the approval of the Administrator. The developer shall maintain these signs in good repair for the duration of the construction of the project. Once construction is complete, the developer shall remove the signs. Dust control can be performed outside the working hours to the approval of the Administrator. A construction vehicular circulation and staging plan shall be submitted for the approval of the Administrator concurrent with the submittal of the initial grading permit for the development. The plan shall indicate proposed vehicle routes that minimize the use of and impacts to public streets.

SECTION V

PLAN PROCESSING AND SUBMITTALS

INTRODUCTION

This section defines the procedures and definitions for submittals for **Permitted Uses** submittals for uses requiring a **Site Plan Review** and submittals for uses requiring a **Special Use Permit**. These procedures and definitions should be used in conjunction with the uses described for each development area within Pioneer Meadows as they are described within **Section III-Development Standards and Descriptions**.

Design Review Process

The design review process is required for all new development and construction as well as exterior remodels and site revisions to projects within Pioneer Meadows. The Pioneer Meadows Development Review Committee, (DRC) established through the Pioneer Meadows CC & R's and City of Sparks shall utilize these Design Standards in their reviews of those proposed projects. The standards within this Handbook apply to and

New construction, remodeling and grading projects in Pioneer Meadows must obtain approval of their Site, Architectural and Landscape plans from the Pioneer Meadows Development Review Committee (DRC) PRIOR to submitting to the City for their review and approval, and for any and all entitlements and permits necessary (i.e. Parcel Maps, Tentative Maps, Final Maps, Site Plan Review, Building Permits, Special Use Permits (SUP) or others as warranted by the City of Sparks.

A written DRC approval letter, or stamped plans, must accompany all submittals to the City of Sparks.

The Administrator shall have responsibility to interpret these standards as provided for in this document and, where the handbook is silent, current City of Sparks ordinances and policies at the time of submission for permit shall apply.

Prior to initiating design work, the Applicant is encouraged to contact and meet individually with both the DRC and the City of Sparks Staff in pre-application conferences to review preliminary plans and determine general conformance with Pioneer Meadow's design intent and permitted uses.

The design review process shall culminate as follows:

City of Sparks Review

Upon receipt of a letter of approval from the DRC, the Applicant shall submit to the City of Sparks for all Parcel Map, Tentative Map, Final Map, Special Use Permit, Site Plan Review and other reviews, permitting and/or entitlements as required by the City of Sparks. The Applicant may not proceed with construction until all such permitting has been approved by the City of Sparks.

Development Standards Handbook for Pioneer Meadows
Section V-Plan Processing and Submittals

Handbook Modifications. Deviations/Waivers and Appeals:

Any proposed amendments or modifications of this Handbook should be submitted to the Administrator. The Administrator will then determine whether it will be reviewed and approved administratively or require an amendment to the Handbook following the process defined in NRS. Nothing contained in the content of this Section shall in any way be construed as placing the City or its Administrator in an appellate capacity in the event of a prior DRC disapproval of any proposed amendment, deviation or variation.

Deviations/Waivers: All requests by an applicant for waivers or deviations from the standards or requirements contained in the Handbook must be approved by the DRC prior to submittal to the City of Sparks for their review and approval.

Deviations/Waivers Approval Procedures and Parameters: The Administrator shall have the authority to process and approve deviations and waivers from these standards consistent with City Code and the Handbook in effect at the time of the request.

Appeals: Any aggrieved party may appeal the decision of the Administrator to the Planning Commission or other body designated by the City of Sparks, and the decision of the Planning Commission to the City Council pursuant to the current provisions of the Code.

City Review

The developer shall submit the final hydrology report and final sewer report, for review and approval by the Engineering Services Manager of Community Development and the Administrator with the submittal of the first final map. Each final map submittal shall include civil improvements plans that include all open space, trail/pathways, and streetscape improvements, internal or adjacent to the area being final mapped in accordance with Sparks Municipal Code and the Handbook.

All development plans to establish new construction and building expansion as defined in the appropriate land use section, will require Site Plan Review and/or Special Use Permit (when applicable) by the City of Sparks, as defined in the description of land uses in this handbook.

Development Review Committee - DRC

The DRC for Pioneer Meadows shall be a committee comprised of at least three individuals who are owners, tenants, or occupants of projects located within Pioneer Meadows, or their designated representatives. The DRC shall be created by the granter CC&R's for the Planned Development.

The DRC shall employ the services of a Nevada licensed architect, landscape architect, engineer, or other professional(s) to advise the DRC as warranted by the DRC.

**Development Standards Handbook for Pioneer Meadows
Section V-Plan Processing and Submittals**

Individual Project Approval Process

General Provisions

Prior to development of an Individual Project within the Pioneer Meadows Planned Development, such Individual Project shall be reviewed to determine that it meets and conforms with the standards and guidelines of the Handbook, following the plan review approval process. In addition, Individual Projects requiring a Special Use Permit must adhere to the normal Sparks Special Use Permit process; provided that the application for a Special Use Permit may be processed concurrently with the Tentative Map, or Site Plan Review for the Phase in which the Individual Project is located and/or the review process and illustrated in **Exhibit V-1**.

A developer or builder of an Individual Project is referred to hereinafter as the "Project Applicant". Prior to submitting plans for an individual Project to the City for review and approval, a Project Applicant shall first obtain approval of the plans from the Pioneer Meadows Architectural Committee (the DRC), pursuant to a review and certification process set forth in covenants and conditions recorded by the Master Developer that encumber the entirety of Pioneer Meadows ("Pioneer Meadows Covenants"). After obtaining such certification, the plans shall be submitted to the City for Tentative Map, Site Plan Review or other actions to assure that each Individual Project meets or exceeds the Development Standards and fits within the Design Parameters established by this Handbook. Refer to **Exhibit V-1** for a graphic illustration of the process.

With regard to all determinations made by the City, the City may grant a deviation of up to twenty percent (20%) from the standards and guidelines in the Handbook without conducting a hearing as long as such deviation will not impair the purpose of the Handbook (each a "Minor Deviation".) Adjustments to the boundaries of such land use designations due to changes in the locations of streets dividing two such designations shall not constitute a relocation of a land use designation.)

Approval of the plans for an Individual Project may be sought either after final handbook approval of the Phase in which the Individual Project is located, or concurrently with the final handbook approval process; provided, however, that in no event shall the Tentative Map or Site Plan Review be completed prior to completion for the final handbook approval process. In the event of any conflict between the process set forth in this Handbook and any other review process used by the City, this Handbook shall control. Where this Handbook is silent on a particular approval procedure, the Sparks Municipal Code or other regulations governing development of land in effect at the time shall prevail.

Applications for commercial and residential Individual Projects are subject to different approval processes and are addressed separately in the following sections. For definition purposes, an "Individual Residential Project" is defined as one that is built as "for sale" residential housing, whether attached or detached; an "Individual Commercial Project" is defined as one that involves commercial, office/business park and multi-family "for rent" projects.

**Development Standards Handbook for Pioneer Meadows
Section V-Plan Processing and Submittals**

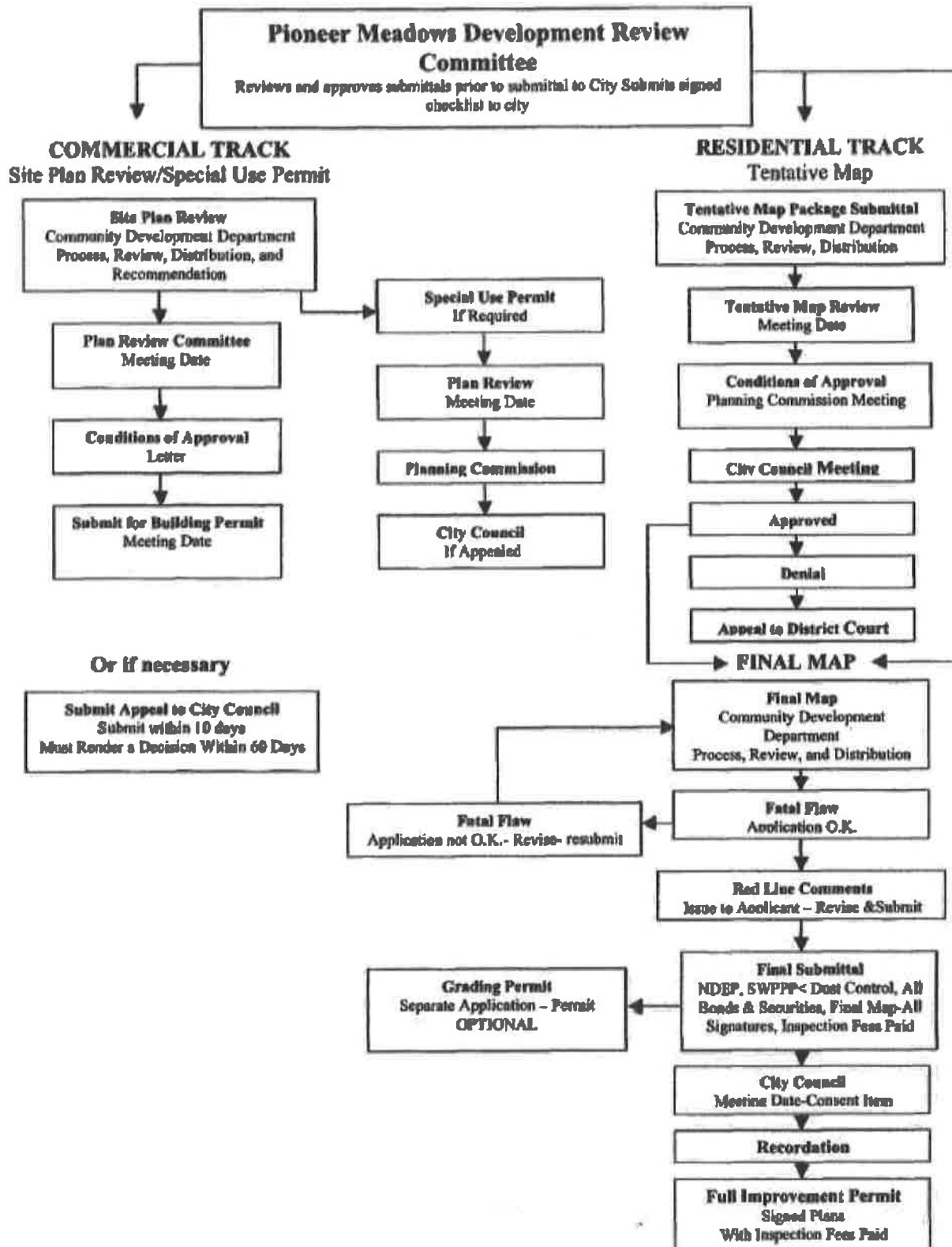
Individual Business/Commercial Project Site Plan Review Approval

All new building construction requires a site plan review, provided that some construction must first be authorized by a Special Use Permit. The review process for Individual Business/Commercial Projects consists of three distinct, but interrelated phases: (refer to **Exhibit V-1**)

- 1) The first phase is the pre-application process. It is during this phase that a Project Applicant becomes familiar with the application requirements and obtains feedback on preliminary design ideas from the DRC and the city.
- 2) The second phase is the DRC review/certification process.
- 3) The third phase consists of the City's process for approving the Individual Business/Commercial Project, including a Special Use Permit approval, if applicable. Here the City will make its determination as to whether the Project meets and conforms to the standards set forth in this Handbook.

The Project Applicant shall become familiar with the City's review process and application requirements. (Refer to **Exhibit V-1**, Individual Project Approval Process - Commercial Track, for Specific steps required for project approval.)

**Development Standards Handbook for Pioneer Meadows
Section V-Plan Processing and Submittals**



**Development Standards Handbook for Pioneer Meadows
Section V-Plan Processing and Submittals**

Residential Project Approval Process

Like the review process for Individual Commercial Projects, the review process for Individual Residential Projects includes review by both the DRC and the City of Sparks. In addition, the review process for Individual Residential Projects includes approval of a Tentative Map and a Final Map. The process consists of three distinct, but inter-related phases:

- 1) The first phase is the pre-application process. It is during this phase that the Project Applicant becomes familiar with the application requirements and obtains feedback on preliminary design ideas from the DRC and the city.
- 2) The second phase consists of both the DRC and the City of Sparks Tentative Map process
- 3) The third phase is the Final Map approval process by the DRC and the City of Sparks.

It is incumbent upon the Project Applicant to become familiar with the DRC and City review process and application requirements. (Refer to **Exhibit V-1** Individual Project Approval Process - Residential Track, for specific steps required for project approval.)

SECTION VI

FINAL DESIGN REQUIREMENTS

FINAL DESIGN REQUIREMENTS

Subdivision Design and Construction

Subdivision design and construction shall conform to the City of Sparks' Standards and the "Orange Book" Standard Specifications for Public Works Construction (latest version) to the approval of the Public Works Director. All plans submitted to the Administrator for approval shall clearly indicate all proposed improvements in both plan and profile. Existing grades and final grades shall be clearly shown on profiles. All designs will be certified by a Nevada Registered Engineer. Each sheet of plans shall carry in the lower right hand corner a title block, which shall contain the name of the subdivision, owners, the type of design shown on the plans, the name of the registered engineer, date, sheet number, total number of sheets and any information necessary to clarify the design.

Each sheet of plans shall have a north arrow and shall indicate the scale used. The plans shall be on standard 24" (twenty-four inch) by 36" (thirty-six inch) or 24" (twenty-four inch) by 32" (thirty-two inch) original or reproducible sheets. A legible 8 1/2" (eight and one half inch) by 11" (eleven inch) copy of each plan shall also be submitted.

All improvement drawings shall be submitted to the Development Operations Division of the Public Works Department, with an approval block for City of Sparks signature and date on each sheet of the Civil Improvement Drawings.

Street Design

Maximum length of cul-de-sac streets shall be 400'. Each cul-de-sac shall be designed with a turnaround which shall have a minimum dedicated right-of-way radius of forty-five feet (45'), with an improved section of not less than thirty-six feet (36').

All streets shall have a minimum grade of three-tenths of one percent (0.3%) where L-type gutters are used. Vertical curves shall be designed into all changes in grade where the algebraic difference is two percent (2%) or more.

No centerline curve radius of less than 100' shall be designed on any street (in lieu of a 100' radius, a knuckle with a minimum radius of forty-five feet (45') can be used).

Street right-of-way widths and street dimensions shall meet the minimum requirements of the Regional Transportation Commission (RTC) to the approval of the Public Works Director. For those roads meeting the criteria for designation as regional roads, the design requirements are described in this Handbook (reference to **Section VII-Infrastructure and Public Services - Table VII-2, Year 2007 Criteria**). Right-of-way widths and street dimensions for other roads within this development shall meet the minimum requirements of the Administrator and as shown in the corresponding figures throughout this Handbook.

Horizontal curves, vertical curves and minimum speed design criteria shall meet the requirements set forth by the Public Works Director and the RTC. Design shall include the provision for adequate sight distance in accordance with standard engineering practice.

**Development Standards Handbook for Pioneer Meadows
Section VI-Final Design Requirements**

Any street or highway intersecting any other street or highway shall intersect at an angle as near to a right angle (90°) as is practicable. Intersections shall not be designed at angles less than 60 degrees (60°).

At each intersection, the property line at each block corner shall be rounded with a curve. This curve will have a radius of not less than twenty feet (20') on collector streets or less than thirty feet (30') on arterial streets.

No street name will be used which will be duplicated or confused with the name of an existing street in the City of Sparks or Washoe County. All street names will be approved by the Washoe County Street Naming Committee.

New streets that are extensions of, or obviously in alignment with, existing streets, shall bear the name of the existing street.

Structural street sections shall be as follows:

- Residential - Four inches (4") asphaltic concrete (AC) and eight inches (8") aggregate base.*
- Collectors - Five inches (5") asphaltic concrete (AC) and eight inches (8") aggregate base.*
- Arterials - Six inches (6") asphaltic concrete (AC) and ten inches (10") aggregate base.*

* Note: Structural street sections may be revised and reduced based upon review of R-values taken during construction on subgrade. These values will be reviewed and approved by the City of Sparks' Public Works Director.

Additional depth of structural section may be required if projected traffic volumes and/or soil conditions warrant a greater structural section.

A site specific Geotechnical and Traffic Report (if deemed necessary by the City of Sparks' Public Works Director) will be provided for each development (submitted to the City of Sparks) for approval by the Public Works Director.

Public Utility Easements

Public utility easements will be provided where required to adequately accommodate all public utilities.

Underground Utilities, Electrical Supply and Street Lights

Street lights within the public right-of-way shall be in accordance with a street light survey. This survey will be provided by the Developer for approval by the Public Works Director prior to the submittal of the first final map and/or public improvement plans. The survey will indicate the type of lighting proposed for each village and for those roads outside of the villages. Standard minimum spacing based upon road and lighting criteria

Development Standards Handbook for Pioneer Meadows
Section VI-Final Design Requirements

shall be established to assist in the design and plan review process. The lighting will be in accordance with Sierra Pacific Power Company standards.

The electrical supply shall be adequate for domestic use and street lighting.

Street lights will be placed on subdivision utility drawings after Sierra Pacific Power Company (or provider) completes the electrical improvement drawings and after review and acceptance of the locations by the City of Sparks' Public Works Director.

The spacing of street lights shall be such that they provide a level of lighting approved by the City of Sparks' Public Works Director.

Water Supply and Fire Hydrants

The water supply shall be adequate for all domestic use and fire protection needs.

Fire hydrants will be located not less than eighteen inches (18") from the back of the curb to the center of the fire hydrant. Locations of hydrants shall be subject to the approval of the City of Sparks' Fire Department and the Public Works Department.

Water lines to hydrants shall conform to recommendations of the National Board of Fire Underwriters and shall be clear of pedestrian lanes.

The minimum domestic service shall not be less than three quarters of an inch (3/4") in diameter.

Sanitary Sewer

Sewage collection and/or treatment shall comply with the laws of the State of Nevada and all local ordinances, shall be subject to the approval of the Public Works Director and shall conform to all City of Sparks' Standards and Specifications.

Sewer facilities within easements between lots will not be allowed within this development, unless a twenty foot (20') all weather access is provided.

Drainage

Each Developer shall provide the necessary means to assure complete drainage of his/her property. State or City drains, natural water courses or constructed tributaries will be used, and shall comply with the requirements as set forth in this section, and the approved Master Drainage Report (reference **Section VII-Infrastructure and Public Services**).

Drainage facilities shall be designed and provided in accordance with the requirements of the approved Master Drainage Report and the City of Sparks' Hydrologic Criteria and Drainage Design Manual. Construction shall be in accordance with the City of Sparks' Standards and the "Orange Book" Standard Specifications for Public Works Construction.

Development Standards Handbook for Pioneer Meadows
Section VI-Final Design Requirements

Backyard drainage facilities, such as drain inlets and swales, will not be allowed within the Single Family residential districts. All lots shall drain to the abutting streets or regional drainage facility.

The Applicant shall submit a development plan for each subdivision clearly showing the drainage for each lot. Each lot will be designed so that water will not run off onto adjacent lots.

Geotechnical

A Geotechnical Report titled "PRELIMINARY GEOTECHNICAL INVESTIGATION - Pace Properties, Spanish Springs, Nevada" dated January 1999 was prepared by Stantec Consulting Inc. and shall be used as a reference and updated with each development of each individual parcel (as described in the cover letter of the report). Each development submitted to the City of Sparks' for review shall address all issues associated with this report and conform to Section 8.0 of the aforementioned report.

SECTION VII

INFRASTRUCTURE AND PUBLIC SERVICES

INFRASTRUCTURE AND PUBLIC SERVICES

Hydrology

The “*Addendum Drainage Master Plan for the Spanish Springs Valley*” was prepared by S.E.A. and financed by the City of Sparks, Pioneer Meadows LLC, Bighorn, Tucker and Wingfield. This analysis serves as a basis for development in the Spanish Springs Valley, defining flow rates, locations of regional drainage facilities, channel sizing and hydraulic grade line connections at property lines. An agreement was formed with the City of Sparks and the aforementioned land owners describing the process each developer would follow for the hydraulic and hydrologic analyses of their respective parcels. This agreement was set up to ensure the City of Sparks that each development would consider its impacts to the valley as a whole and assure a working system at full buildout. *Pioneer Meadows* will follow the procedures set forth in this agreement, incorporating this development into the “*Addendum Drainage Master Plan for the Spanish Springs Valley*”, demonstrating no adverse effects on the Spanish Springs Detention Facility and ensuring upstream flows are mitigated in conformance with the guidelines set forth. The report demonstrated that no onsite detention will be necessary to control the proposed flows from this development. All flows generated from this development will be conveyed to the regional detention facility or the Pioneer Meadows pond by approved methods as discussed in **Section VI-Final Design Requirements**. The report also discusses the removal of material from the detention facility. This material will be removed in order to increase the volume of the detention facility, and for fill material throughout the site.

A Master Drainage Report will be provided prior to or concurrent with the first development for the approval of the City of Sparks’ Public Works Director. The report will be provided to facilitate the understanding and review of subsequent individual drainage plans. The Master Drainage Report will identify the existing conditions, impacts to the existing conditions and mitigation with sufficient details so as to identify the requirements of each individual unit.

As part of the Master Drainage Plan, a phasing plan will be provided which will identify the sequence of construction (both onsite and offsite storm drainage improvements) as related to the overall site development. The approved phasing plan will be adhered to such that improvement plans will not be submitted for approval until key elements of the drainage system, as determined by the Public Works Director, are constructed.

Pioneer Meadows is currently located within Federal Emergency Management Agency (FEMA) Flood Zones A, AO and C. *Pioneer Meadows* will prepare a Conditional Letter of Map Revision (CLOMR) shortly after the submittal of the Master Drainage Report and submit to the proper agencies. Upon the submittal of the CLOMR and acceptance of the Master Drainage Report, improvements will begin. Once the final drainage improvements are constructed and certified, a Letter of Map Revision (LOMR) will be prepared and submitted to the proper agencies. The LOMR will be done in phases as opposed to one single submittal. This will ensure timely achievement of the removal of the associated flood zones for that particular site.

**Development Standards Handbook for Pioneer Meadows
Section VII-Infrastructure and Public Services**

Temporary drainage facilities that may be proposed shall be designed in accordance with permanent facilities, and constructed utilizing alternative methods subject to the approval of the Public Works Director.

Sewer Demands and Disposals

The following calculations in **Table VII-1** reveal the total sewer outflow from *Pioneer Meadows*:

**TABLE VII-1
TOTAL SEWER CONTRIBUTION OF PIONEER MEADOWS**

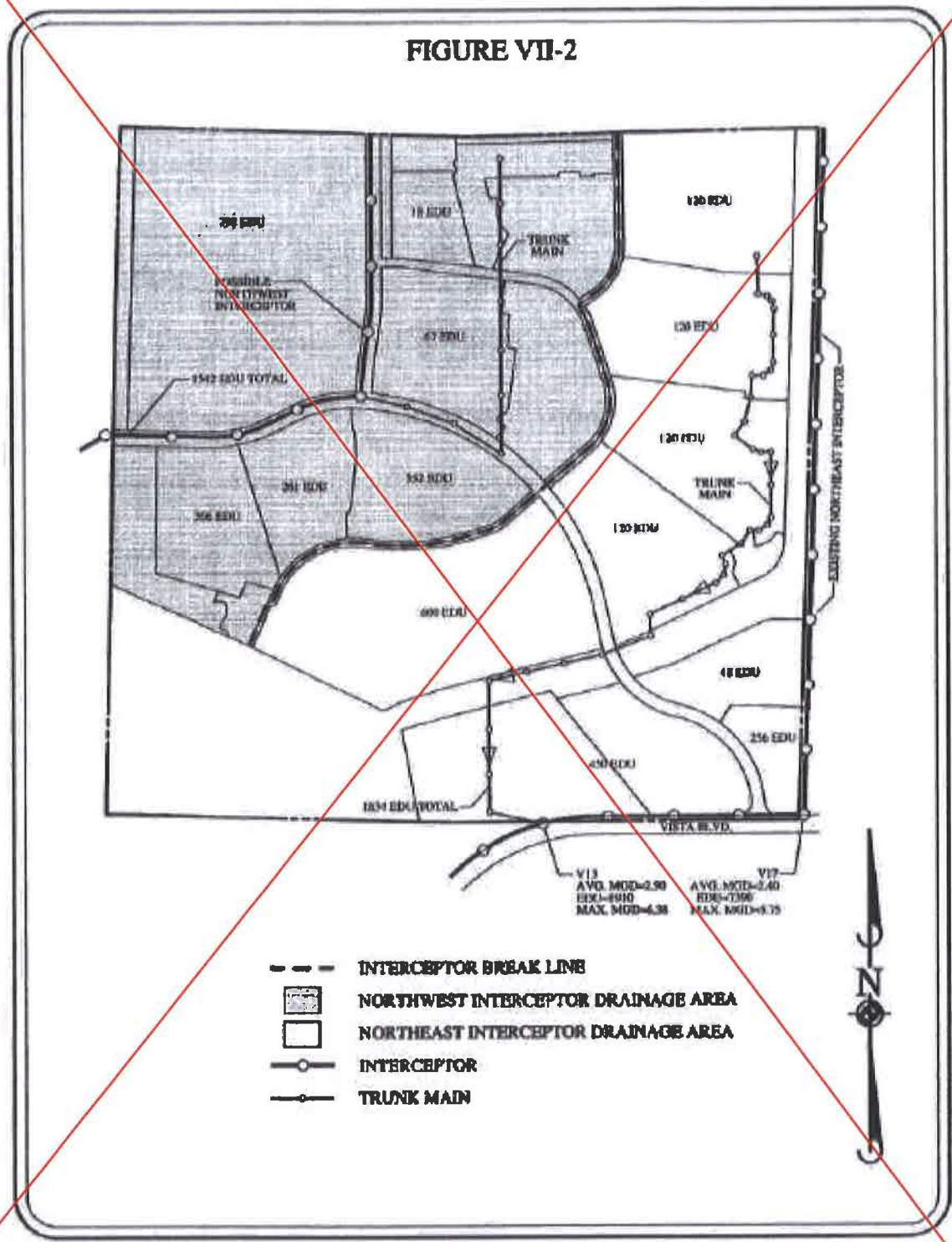
| CLASSIFICATION | ACRES (AC), UNITS (UN) OR STUDENTS | CONTRIBUTION PER AC/DAY OR PER UN/DAY | TOTAL GALLONS/DAY | EQUIVALENT DWELLING UNITS (EDU'S) |
|---|--|---|---------------------------------------|---|
| SCHOOL SITE | 600 STUDENTS | 10 /GAL/STUDENT/DAY | 6,000 | 18 |
| GC | 25 AC | 3,330/AC/DAY | 83,250 | 257 |
| BP | 102 AC | 1,100/AC/DAY | 112,200 | 346 |
| MF15 | 1,369 UN | 325/UN/DAY | 444,925 | 1,369 |
| RD10A | 600 UN | 325/UN/DAY | 195,000 | 600 |
| SFR4 | 781 <u>843</u> UN | 325/UN/DAY | 253,825 <u>273,975</u> | 781 <u>843</u> |
| TOTAL(gallons/day)= | | | 1,089,200 <u>1,109,350</u> | |
| TOTAL(average mgd)= | | | 1.09 <u>1.11</u> | |
| TOTAL(with peaking factor of 2) (gallons/day)= | | | 2,178,400 <u>2,218,700</u> | |
| TOTAL(with peaking factor of 2) (mgd)= | | | 2.18 <u>2.22</u> | |

In the Northern Sparks Sphere of Influence (NSSOI), two (2) sanitary sewer interceptors are planned, both of which will be utilized in sewerage the *Pioneer Meadows* site. The interceptors are known as the Northeast Interceptor and the Northwest Interceptor. The Northwest Interceptor is currently in the planning stages, while the Northeast Interceptor is already constructed. The interceptors were planned within the *Wastewater Facilities Plan and Environmental assessment for the Spanish Springs Valley*, prepared for the City of Sparks by Pyramid Engineers of Reno. *Pioneer Meadows* was assumed to outflow 1520 Equivalent Dwelling Units (EDU's) (0.988 million gallons per day (mgd)) into the Northeast Interceptor, determined by calculating the difference in EDU's between Manholes V13 and V17, with the remaining outflow to the Northwest Interceptor. **Figures VII-1 and VII-2** show Manholes V13 and V17, and depict alternatives for the sewerage of this project. Alternative 1 (**Figure VII-1**) allows a peak flow rate of 1520 EDU (approximately 0.99 mgd) into the Northeast Interceptor with the remaining peak flow rate of ~~1856EDU~~1895EDU (approximately ~~1.21~~23 mgd) flowing into the Northwest Interceptor. Alternative 2, the preferred alternative, (**Figure VII-2**) allows a peak flow rate of 1834 EDU (approximately 1.19 mgd) into the Northeast Interceptor with the remaining peak flow rate of ~~1542EDU~~1581EDU (approximately ~~1.00~~1.03 mgd) flowing into the Northwest Interceptor. The flow rates in excess of the 1520 EDU within the Northeast Interceptor in Alternative 2 were determined by calculating the difference in EDU's between Manholes V13 and V17 (1520

Development Standards Handbook for Pioneer Meadows
Section VII-Infrastructure and Public Services

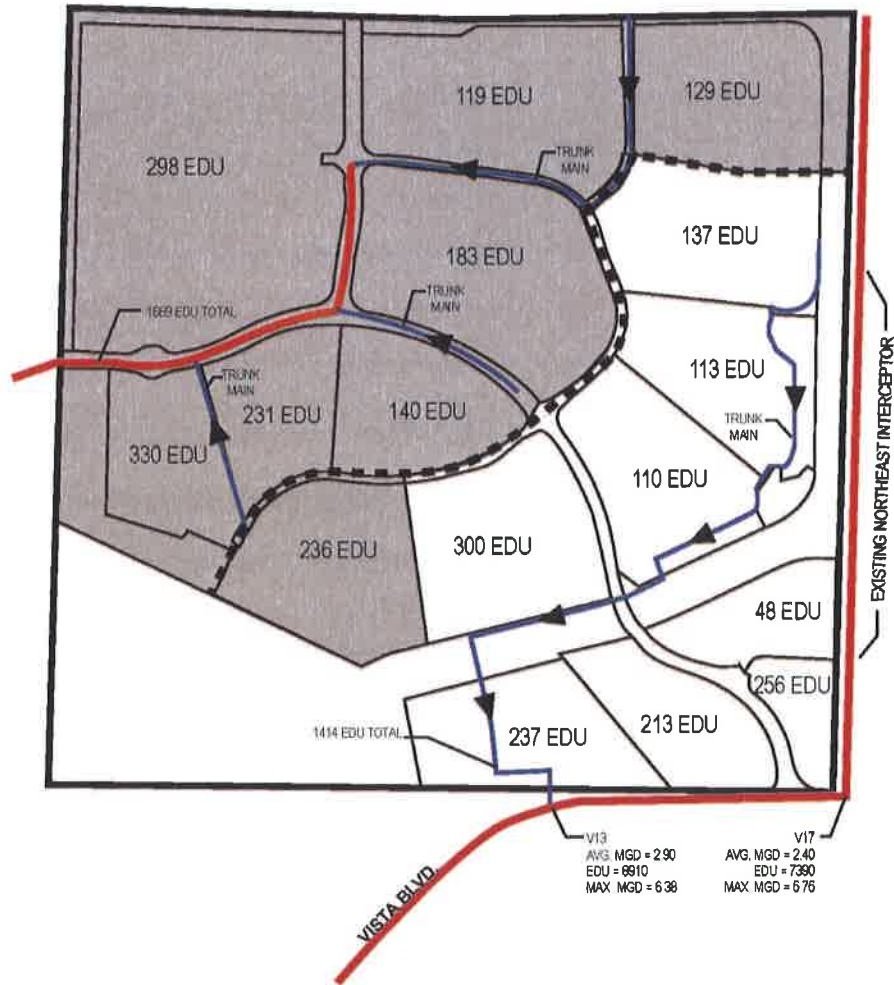
EDU = 0.99 mgd), subtracting this from the peak calculated outflow at Manhole V13 (average flow rate of 2.90 mgd times a peaking

FIGURE VII-2



**Development Standards Handbook for Pioneer Meadows
Section VII-Infrastructure and Public Services**

FIGURE VII-1

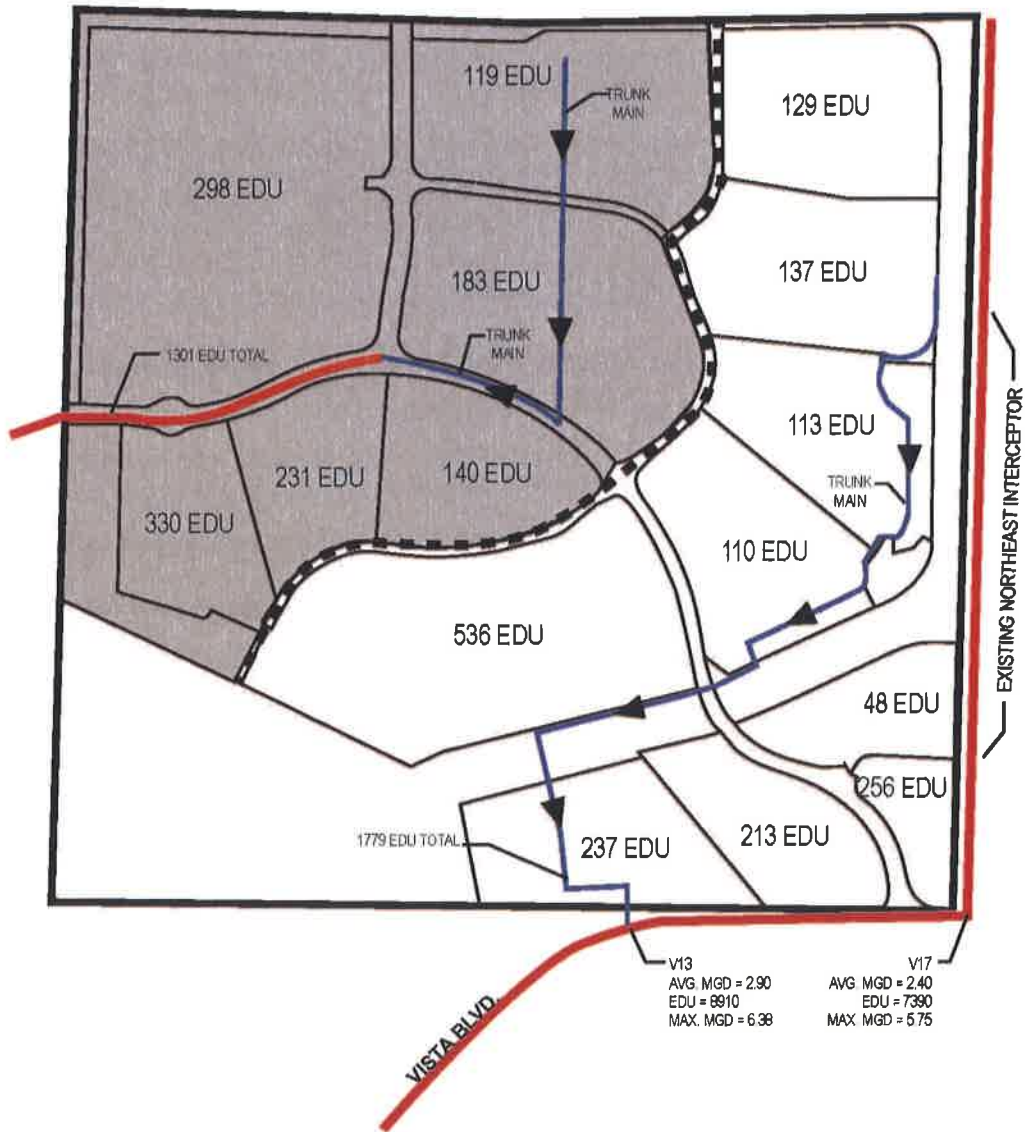


- INTERCEPTOR BREAK LINE
- NORTHWEST INTERCEPTOR DRAINAGE AREA
- NORTHEAST INTERCEPTOR DRAINAGE AREA
- INTERCEPTOR
- TRUNK MAIN



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FIGURE VII-2



- INTERCEPTOR BREAK LINE
- NORTHWEST INTERCEPTOR DRAINAGE AREA
- NORTHEAST INTERCEPTOR DRAINAGE AREA
- INTERCEPTOR
- TRUNK MAIN



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factor of two (2) for a peak outflow of 5.80 mgd) for a total of 4.81 mgd. The maximum capacity of the sewer line downstream of Manhole V13 is 6.38 mgd. This leaves an excess capacity in the line of 1.57 mgd. The use of this additional capacity will be justified by a final sanitary sewer report, which will be prepared prior to or concurrent with the first submittal, and will detail the locations and sizes of the mains, design flow rates, capacities and points of connection into the interceptors.

Grading

This section will apply to all residential and non-residential developments. Areas located within the Common Area but not affected by any land disturbing activities will remain in their natural state. Areas located within the Common Area where land is disturbed will be stabilized as follows:

- Planting shall be designed to and shall achieve eighty percent (80%) ground coverage of any disturbed area within three (3) years.
- Slopes located within the residential lots for this development will not exceed 3:1 (horizontal to vertical). All slopes shall have a solution presented that stabilizes the slope. Erosion control applications for slope treatment shall be provided to the approval of the Public Works Director.
- No grading or clearing of any kind may be initiated until a grading permit is issued associated with an approved development or grading plan. Stock piling of material will not be allowed unless indicated on the grading permit and approved by the Administrator.
- Grading of the residential lots will be accomplished as required for the construction of homes or buildings. Elevation differences between adjoining lots will be taken up by using 3:1 or less side yard slope, or walls.
- Residential lot grading shall conform to the drainage requirements outlined in **Section VI-Final Design Requirements**. Single family residential lot grading shall be designed such that all drainage will be directed to the abutting street or regional drainage facilities.

Arterials and Common Area

Pioneer Meadows is serviced by two (2) arterials and two (2) local collectors (reference to **Figure VII-3** for vehicular circulation plan). The NSSOI Area Plan requires developments to construct a twenty-five (25') landscape buffer along each side of the arterials. *Pioneer Meadows* has set aside a twenty-five foot (25') landscape buffer along both sides of the arterials, a five foot (5') wide meandering sidewalk on both sides, two (2) - five foot (5') wide bike lanes in either direction and a fourteen foot (14') landscaped median in the center of the arterials. The residential collectors within this development are designed to include a four foot (4') wide Portland Cement Concrete (PCC) sidewalk on both sides, eight foot (8') landscape buffers on both sides and two (2) five foot (5') wide bike lanes in either direction. All bike lanes will be located within the right-of-way and will be striped and marked. All landscaped areas located along the arterials and residential collectors will conform to the

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landscaping requirements set forth within this Handbook, and be maintained by the Homeowners' Association (reference to **Section VIII-Covenants, Conditions and Restrictions**). *Pioneer Meadows* will include three (3) plus miles of paths and trails throughout the site. No motorized vehicles will be allowed in these areas (reference to sidewalk, trails, paths, bike lanes and pedestrian circulation figures for material types). All walking paths and trails will be maintained by the Homeowners' Association. All bike lanes, sidewalks and regional trails shall be maintained by the City of Sparks (reference to **Figure VII-7**).

The Applicant, or its agent, reserves the right to construct and/or place any public utilities and/or improvements, excavate fill material, place material, disturb or grade within the designated Common Areas without the consent of the Homeowners' Association.

Entries

Pioneer Meadows Homeowners' Association will maintain a visually pleasing environment, incorporating landscaping into the entry ways into all residential and non-residential developments.

The main entrance into *Pioneer Meadows* is located on Vista Boulevard. This entrance includes left and right ingress and egress turn pockets and a fourteen foot (14') center median, all of which will be landscaped with a mixture of ground cover, trees, shrubs and lawn. All collector street entrances off of arterials shall be designed with a one foot (1') wide concrete median and have two (2) eleven foot (11') lanes to allow left and right ingress and egress.

Each village shall have a minimum of one (1) primary entry. All residential and non-residential **primary** entries (collector streets to local streets) shall be designed with a ten foot (10') landscaped median split along the center line, two twenty-four foot (24') paved sections and a ten foot (10') landscape buffer along all corner lots (reference to **Figures VII-5 and VII-6**). Monument signs located within the landscaped area at each of these entries will designate the name of the development, business or village. Monument signs shall not be located within the medians.

Village entry statements and landscaping must go through the Design Review Committee (DRC) process identified for non-residential architectural standards.

Pedestrian Circulation

Pioneer Meadows will develop a pedestrian circulation plan within this development. The following depict the general criteria and development standards:

- If the path is in the right-of-way or is functioning as the public sidewalk, the path shall be concrete and designated "sidewalk" within this Handbook.
- If the path is functioning as a regional trail, it shall be ten feet (10') wide (minimum) and designated "regional trail" within this Handbook.

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1. If the path is located within or contiguous to the right-of-way, it shall be concrete.
 2. If the path is located within the Common Area, it shall be asphaltic concrete (AC).
- If the path is privately maintained and not functioning as a public sidewalk, it shall be a minimum of five feet (5') in width and designated "path" within this Handbook.
 1. Paths will only be allowed in the Common Areas.
 2. The paths will be constructed of fine (sandy) native material found onsite, compacted and graded to achieve a smooth walking surface.
 - Exterior walks shall be located along the length of Vista Boulevard from the western property line to the eastern property line.
 1. All exterior walks shall be eight feet (8') in width.
 2. All exterior walks shall be constructed of concrete.
 - At grade pedestrian crossings across arterials will be constructed with pedestrian activated walking devices subject to the approval of the Public Works Director.
 - At grade pedestrian crossings across local collectors will be striped with crosswalks.
 - Full continuous pedestrian access shall be provided throughout the entire development. As a minimum, sidewalks and connecting ramps for the physically disabled shall be provided on both sides of all public streets within the right-of-way. Connecting ramps into the trails and paths shall also be provided. Sidewalks and connecting ramps for the disabled shall be provided on one side of all private streets. All sidewalks, trails and paths throughout this development shall be constructed in accordance with the above standards. Within private streets, the Developer may submit an alternate pedestrian access plan for review and approval by the Administrator.

Sidewalks, Trails, Paths and Bike Lanes

Pioneer Meadows interconnects the trails, paths, bike lanes and sidewalks throughout the site (see **Figure VII-7**). Internal paths within the residential developments will be required to connect to the trail systems. These connections will be reviewed and approved by the Administrator. The following describes the locations of each facility and the type of facility:

- **Bike Lanes:** The bike lanes are located on all the arterials and residential collectors. They will be five feet (5') in width on each side of the roadway within the paved section.
- **Sidewalks:** The sidewalks will be constructed along the arterials and residential collectors on both sides. The sidewalks located along the arterials shall be five feet

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(5') in width and located on both sides. These sidewalks will meander throughout the landscape strip. The sidewalks located along the residential collectors will vary in width from five feet (5') along non-residential districts to four feet (4') along residential districts. The sidewalks will be located behind the curb. All sidewalks constructed within the individual residential districts located within the right-of-way shall be four feet (4') in width and constructed behind the back face of curb. All sidewalks constructed within the individual non-residential districts located within the right-of-way shall be five feet (5') in width constructed behind the back face of curb. All designated sidewalks within this development will be constructed of Portland Cement Concrete (PCC) (reference to **Section VI-Final Design Requirements**).

- Regional Trails: *Pioneer Meadows* includes three (3) plus miles of trails generally located along the exterior of the development. The trails will connect the regional park with Kiley Ranch and Wingfield Springs in accordance with the Regional Parks Trail System. This development will set aside a twenty foot (20') access easement for the development of this Regional Trail. ~~The exception to the 20' easement is a 55' easement set aside along the northern portion of the site to the east of Kiley Ranch Drive.~~ Within the twenty foot (20') easement, a ten foot (10') asphaltic concrete (AC) walk will be constructed. The Regional Trail will terminate into a park located on the ~~southeast~~ southwest border of *Pioneer Meadows*, which is contiguous to the ~~fourteen (14)~~ twenty two (22) acre ~~BP parcel~~ Village 12 MF15 parcel. The landscaping along this portion of the trail shall reflect and enhance the park landscaping. All walking trails will be maintained by the Homeowners' Association. Please reference to **Figure VII-7, VII-8 and VII-9**.
- Paths: *Pioneer Meadows* will develop an internal path system that connects the Regional Trail system with the sidewalks, bike lanes and residential and non-residential developments. These paths may be associated with a drainage system that will convey portions of the developed flows into the major drainage facilities. These paths and drainage facilities will be private and incorporated into the Common Areas within this development, and maintained by the HOA. The drainage portion of these areas will be constructed in accordance with the standards described within this Handbook. The paths will vary in width, but in no case be less than five feet (5') in width. The paths will be constructed of fine (sandy) native material found on site, compacted and graded to achieve a smooth walking surface (reference to **Figure VII-7**).
- Exterior Walks: Exterior walks within this development are located adjacent to Vista Boulevard. These exterior walks will be constructed with the developments contiguous to Vista Boulevard, within their respective boundaries.
- The designated "regional trails" within *Pioneer Meadows* are designed in accordance with the NSSOI Area Plan and are subject to reimbursement through the NSSOI development fees. Since the trails are regional and will be used by patrons other than residents of *Pioneer Meadows*, there will be maintenance issues associated with these "regional trails". The reimbursement and maintenance issues will be addressed under a separate agreement.

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Vehicle Circulation Plan

There are five different street sections associated with this development, all of which are generally standard sections. They are as follows:

- Arterials - Public right-of-way (reference to attached cross section-**Figure VII-10**).
- Residential collector streets - Public right-of-way (reference to attached cross section-**Figure VII-11**).
- Local streets (two (2) different sections) - Public right-of-way (reference to attached cross sections-**Figures VII-12 and VII-13**).
- Private streets - Private right-of-way (reference to attached cross section and plan view-**Figures VII-14 and VII-15**).

Arterials (Public)

There will be two arterials associated with the *Pioneer Meadows* project. The arterials are designated within the Capital Improvement Plan (CIP) developed by the Regional Transportation Commission (RTC), as described in the Regional Road Impact Fee System (RRIFS). The arterials are designed in conformance with the current standards set forth by the RTC. Prior to any construction on the arterials, the Developer or Builder will dedicate the appropriate right-of-way to the City of Sparks in accordance with the RRIFS, and enter into a Capital Contribution Front Ending Agreement (CCFEA) with the RTC. The arterials within this project are designated in the CIP as shown in **Table VII-2**.

**TABLE VII-2
REGIONAL ROAD IMPACT FEE SYSTEM
CAPITAL IMPROVEMENT PLAN (CIP)**

| PIONEER MEADOWS DESIGNATION | CIP DESIGNATION | EXISTING # OF LANES | 2007 - # OF LANES | 2015 - # OF LANES | BUILD OUT |
|-----------------------------|------------------|---------------------|-------------------|-------------------|-----------|
| EAST-WEST ARTERIAL | HILLS DRIVE | 0 | 2 | 4 | 4 |
| NORTH-SOUTH ARTERIAL | KILEY RANCH ROAD | 0 | 4 | 4 | 4 |

| THRESHOLD ADT FOR LANE WIDTHS | | | |
|-------------------------------|----------|---------------|--------|
| AVERAGE DAILY TRAFFIC | 0-10,030 | 10,030-21,660 | 21,660 |
| LANE WIDTH | 2 | 4 | 6 |
| | | | |

The constructed sections for the two arterials associated with this project can be described as follows:

- East-West Arterial (Hills Drive): Hills Drive is designated to be a four (4) lane minor arterial in a 120' right-of-way (reference to **Figure VII-10**) at full buildout. During the

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course of construction of *Pioneer Meadows*, a two (2) lane roadway will suffice until the year 2015. This arterial will be constructed from the outside lanes to the inside lanes. Each interior development will construct the arterial to a minimum of one (1) lane in each direction on each outside portion of the right-of-way along the exterior boundary line of the individual development to properly service the site (reference to **Figure VII-16**). The number of additional lanes constructed in conjunction with the interior developments will be determined by the traffic counts at the time of development, based upon the information provided in **Table VII-2**.

- North-South Arterial (Kiley Ranch Drive): Kiley Ranch Drive is designated to be a four (4) lane minor arterial in a 120' right-of-way (reference to **Figure VII-10**) at full buildout. During the course of construction of *Pioneer Meadows*, a four (4) lane roadway will suffice until the year 2015. This arterial will be constructed from the outside lanes to the inside lanes. Each interior development will construct the arterial to a minimum two (2) lanes in each direction on each outside portion of the right-of-way along the exterior boundary line of the individual development to properly service the site (reference to **Figure VII-16**). The number of additional lanes constructed in conjunction with the interior developments will be determined by the traffic counts at the time of development, based upon the information provided in **Table VII-2**.

Residential Collectors

The residential collectors are designed in conformance with the RTC's standard sections for a two (2) lane collector. The travel lanes are twelve foot (12') in width with a five foot (5') bike lane for a total thirty-four foot (34') paved section. The collectors are unloaded (no driveways) and no parking will be permitted in these sections. The collectors may qualify as regional roads if the minimum traffic counts are met. This will be addressed prior to the construction of the individual collectors.

Local Streets

Two (2) local street sections will be required to service this development: a fifty-five foot (55') right-of-way for standard streets and a forty-six foot (46') right-of-way for cul-de-sacs less than 400' in length. Both sections are in conformance with the standards set forth by the City of Sparks.

Private streets

Residential and non-residential developments may request to the Administrator to utilize private streets within their respective parcels. The private streets will have a minimum width of not less than twenty-nine feet (29'), which is the current minimum for fire access in the City of Sparks.

Design associated with private streets and driveways, particularly those associated with cluster type developments, shall demonstrate that safe vehicular and pedestrian interaction and movement can occur. On private streets, parking shall not be allowed (within the twenty-nine feet (29')). Street lights, storm drain and sanitary sewer facilities shall be

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designed and constructed to the City of Sparks' Standard Specifications for Public Works Construction and shall be privately maintained.

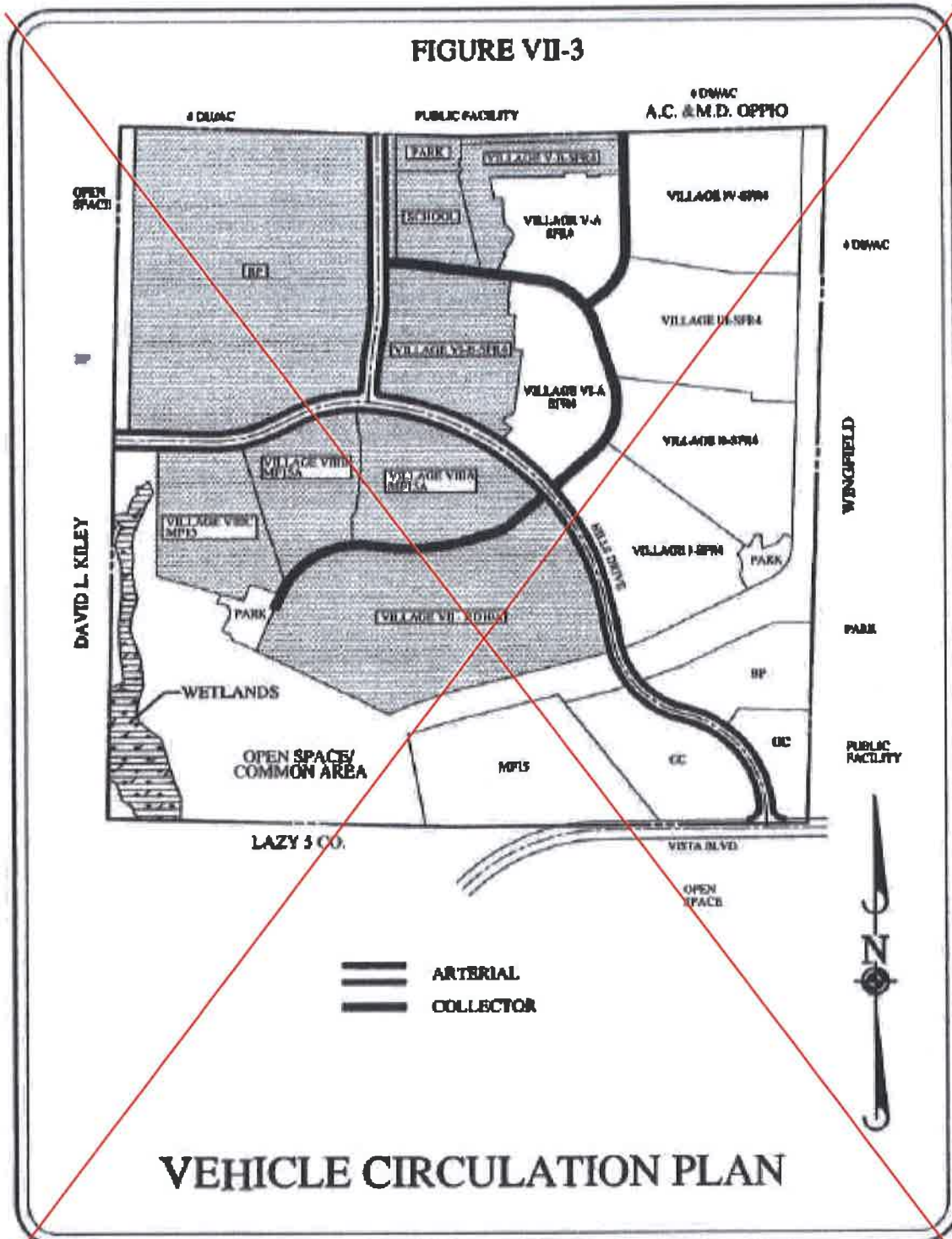
Developments proposing to utilize private streets shall submit a maintenance plan for the proposed private streets and utilities for review and approval by the Administrator. The plan shall include a maintenance schedule at a level equivalent to that currently employed by the City of Sparks for public streets and utilities. The plan shall also include estimated costs to perform the necessary maintenance. The plan will be submitted and approved prior to recordation. The DRC shall update the maintenance schedule on an annual basis to the satisfaction of the Administrator.

Intersections

The east-west arterial (Hills Drive) will be constructed to full width (reference **Figure VII-10**) to the entrance of the multi-family site, then taper down to the required lane widths. The intersection of Vista Boulevard and Hills Drive will be constructed in accordance with **Figure VII-4**. Improvements will be completed to the approval of the Public Works Director.

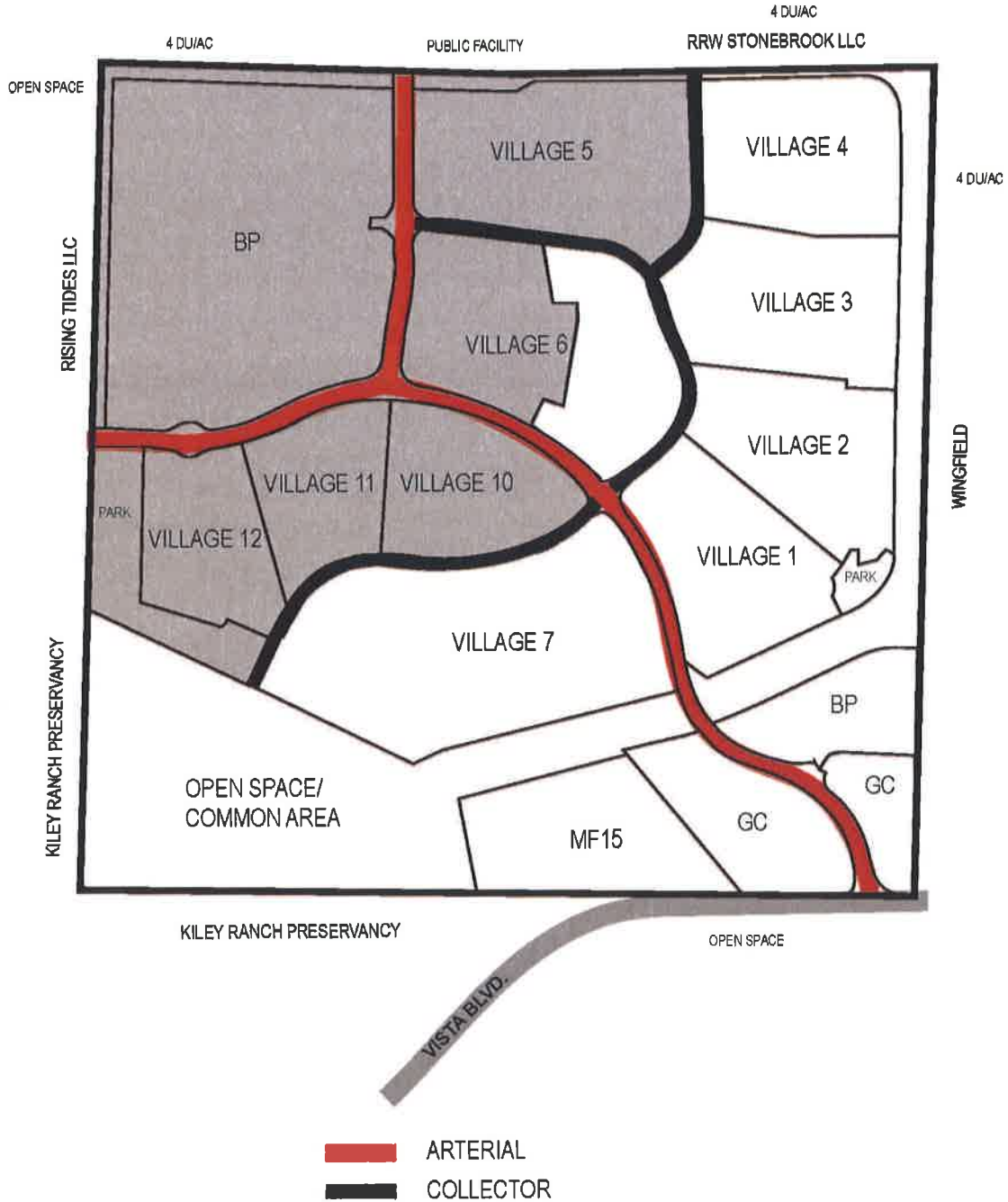
Intersection improvement within the developments that connects to the arterials will be completed to achieve safe turning movements in accordance with the traffic report submitted for the individual development, and to the approval of the Public Works Director.

The development shall be limited to ~~one (1)~~ four (4) way-signalized intersections on Vista Boulevard and Wingfield Hills Road to the approval of the Public Works Director.



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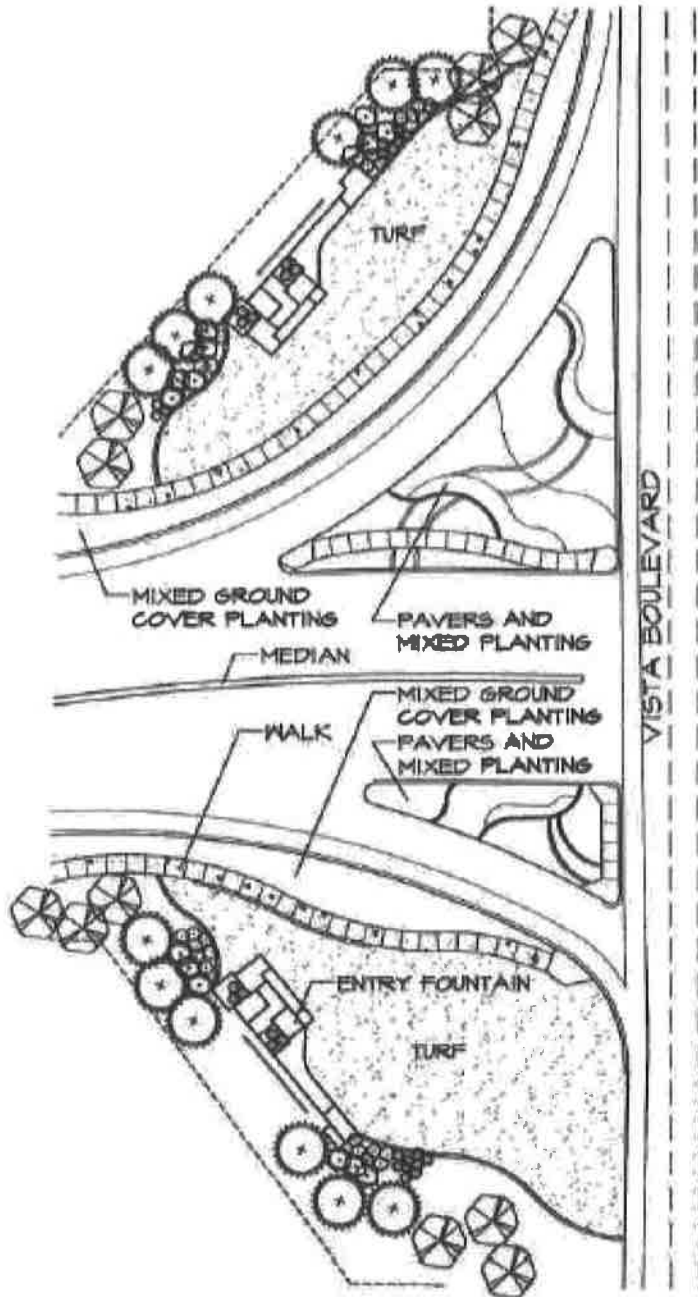
FIGURE VII-3



VEHICLE CIRCULATION PLAN

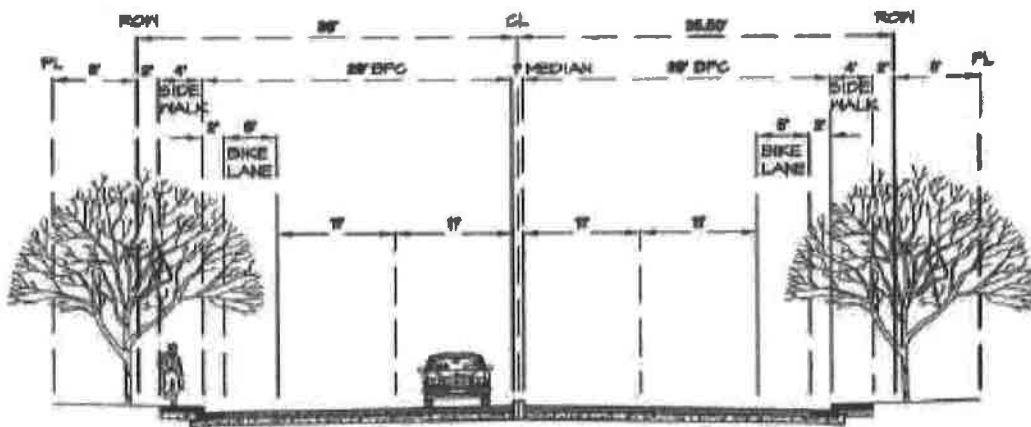
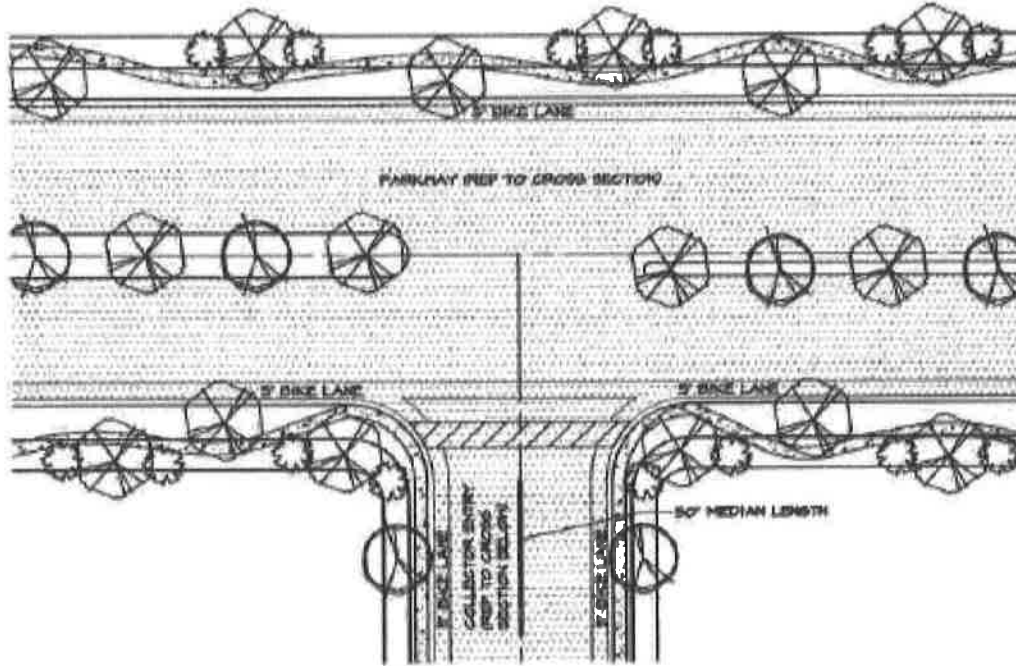


FIGURE VII-4



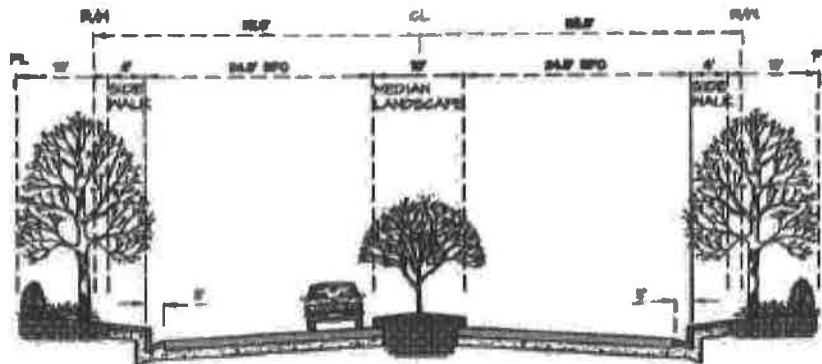
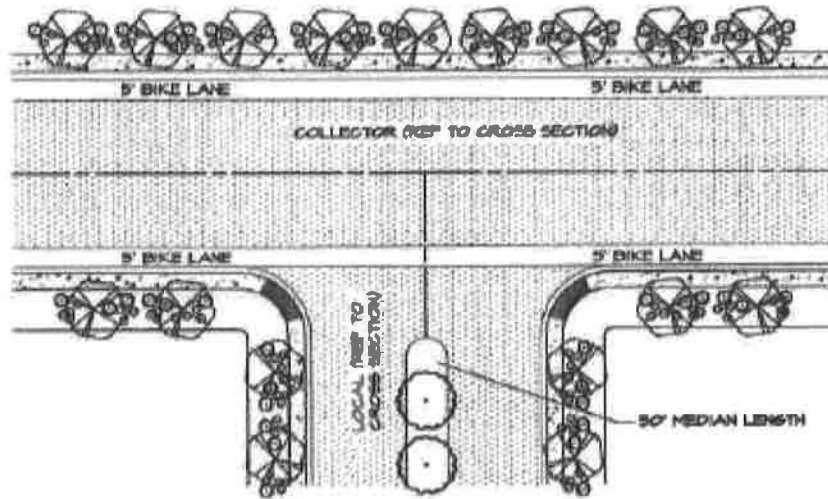
ARTERIAL / VISTA BOULEVARD INTERSECTION

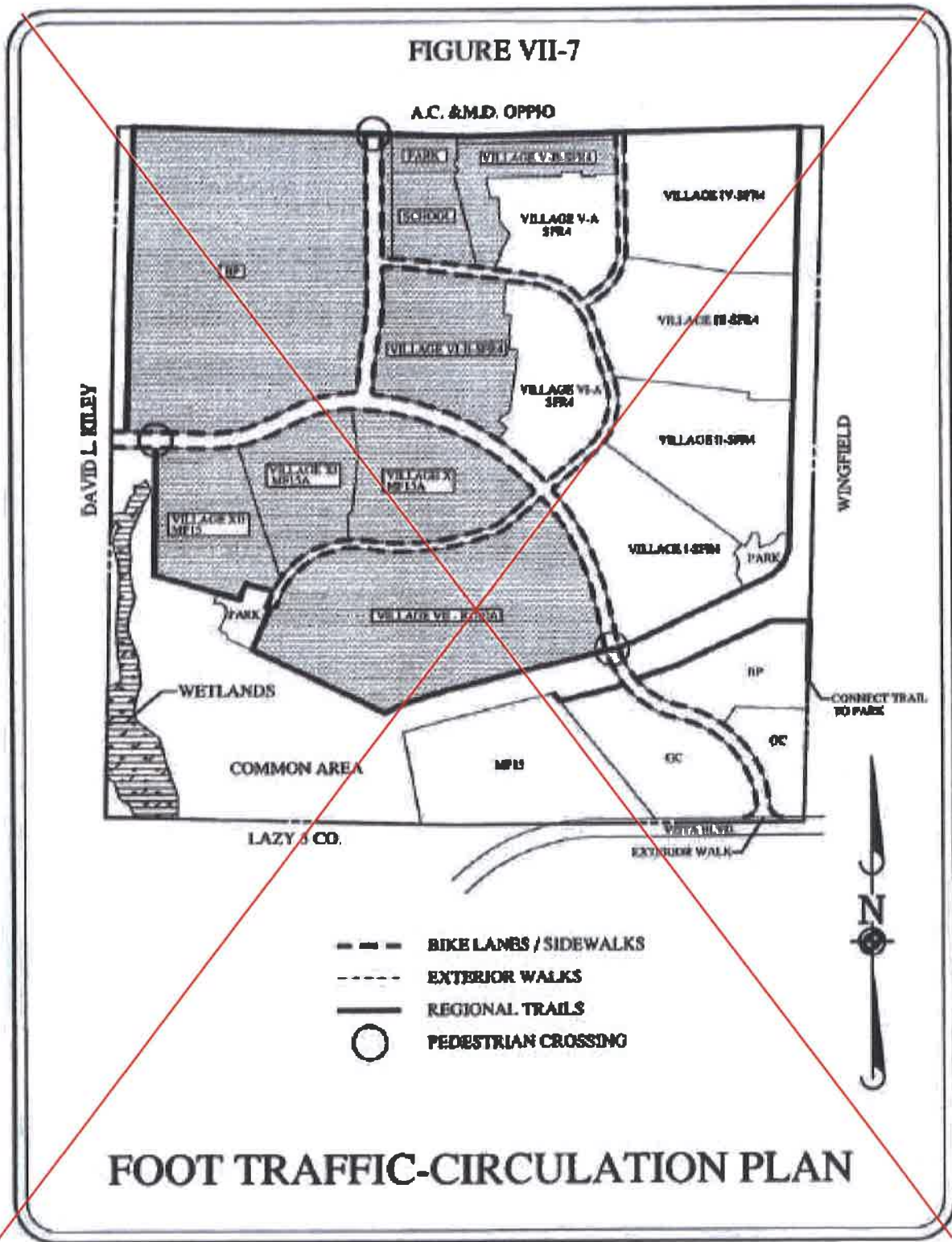
FIGURE VII-5



ARTERIAL TO COLLECTOR
ENTRY WAY (TYPICAL)

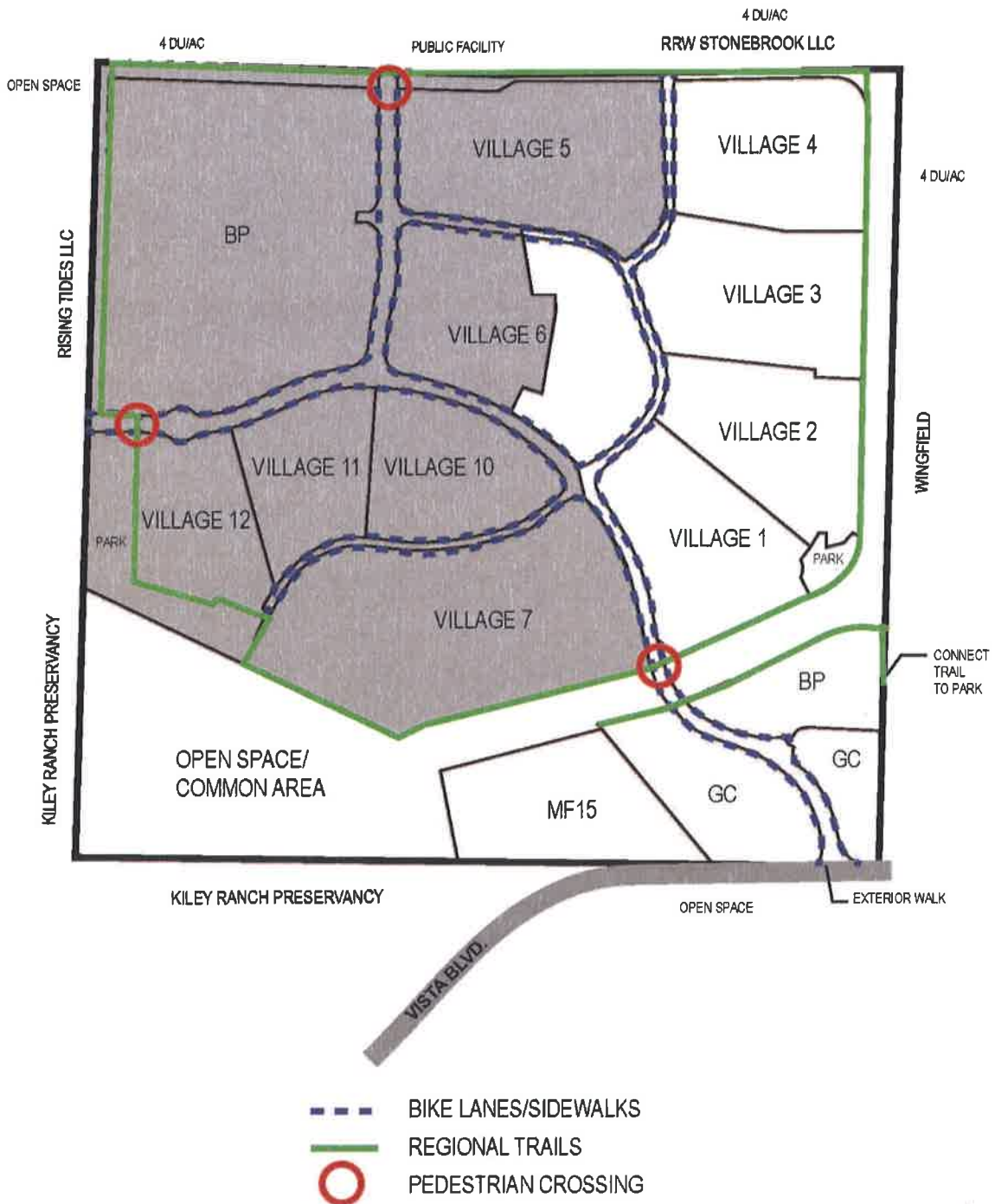
FIGURE VII - 6
COLLECTOR TO LOCAL ENTRY WAY (TYP)





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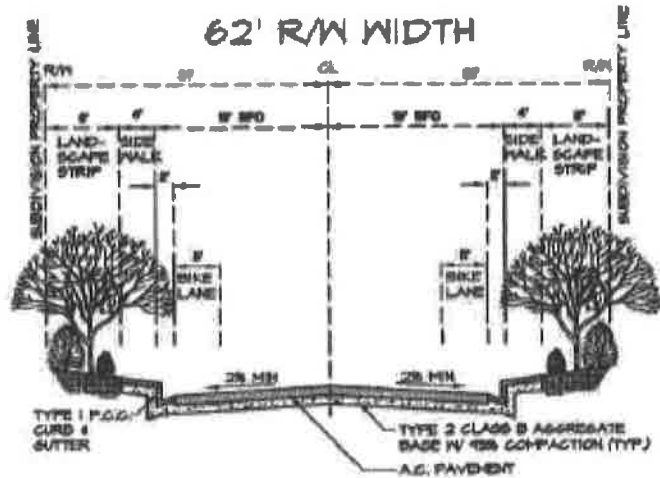
FIGURE VII-4



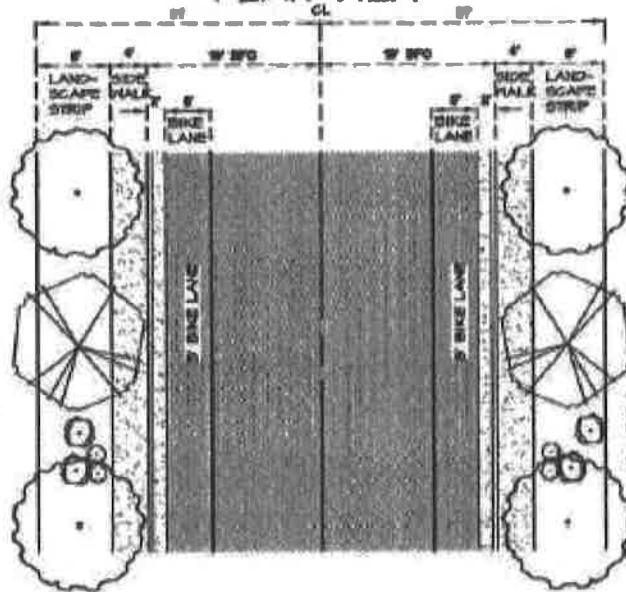
FOOT-TRAFFIC CIRCULATION PLAN



FIGURE VII - 11
COLLECTOR STREET STANDARDS



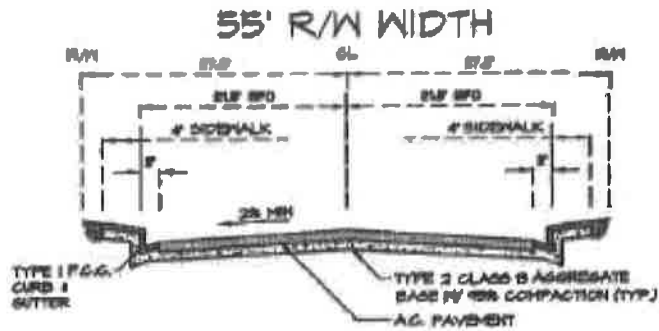
62' R/W STANDARD
PLAN VIEW



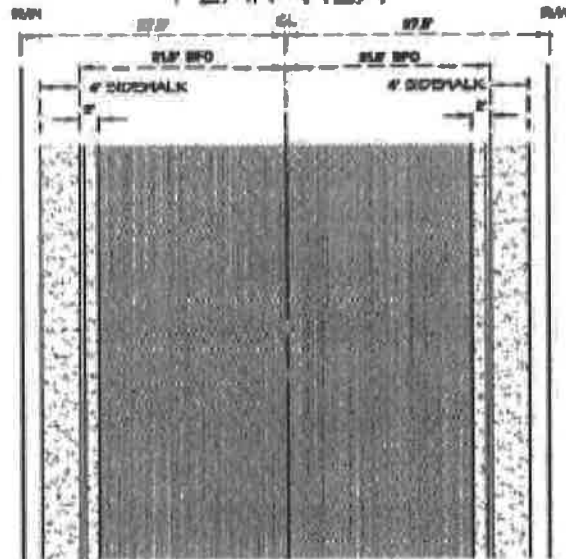
** NOTE:
PAVEMENT SECTION TO
BE CONSTRUCTED IN
ACCORDANCE WITH
SPECIFICATIONS AS
OUTLINED IN SECTION
VI-FINAL DESIGN
REQUIREMENTS.

** NOTE:
SIDEWALK ALONG
NON-RESIDENTIAL
ZONING
CLASSIFICATIONS
TO 8' IN WIDTH

FIGURE VII - 12
LOCAL STREET SECTIONS

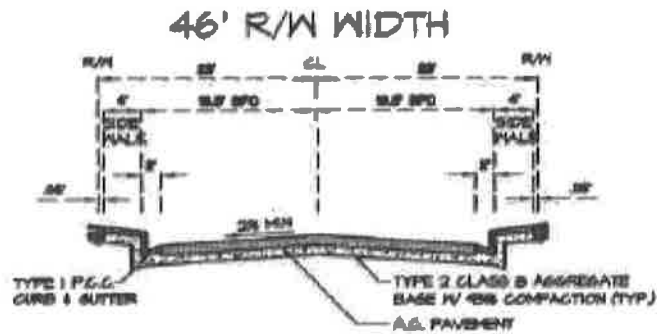


55' R/W STANDARD
PLAN VIEW

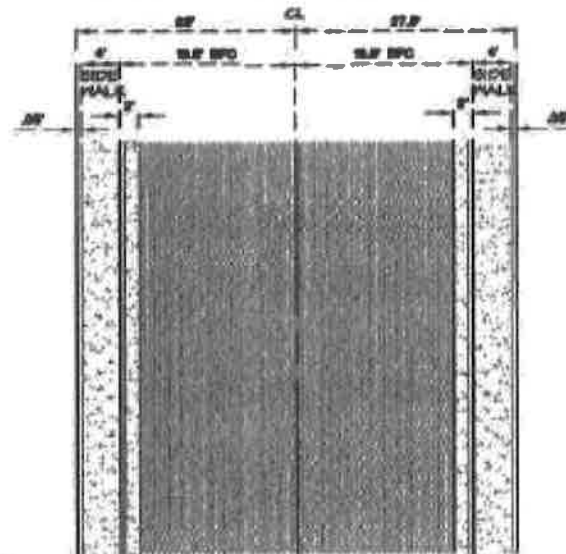


** NOTE:
PAVEMENT SECTION TO BE CONSTRUCTED IN ACCORDANCE WITH
SPECIFICATIONS AS OUTLINED IN SECTION VI-FINAL DESIGN REQUIREMENTS

FIGURE VII - 13
LOCAL STREET SECTIONS



CUL-DE-SACS LESS THAN 400' IN LENGTH



** NOTE:
PAVEMENT SECTION TO BE CONSTRUCTED IN ACCORDANCE WITH
SPECIFICATIONS AS OUTLINED IN SECTION VI-FINAL DESIGN REQUIREMENTS

FIGURE VII - 14
PRIVATE DRIVE

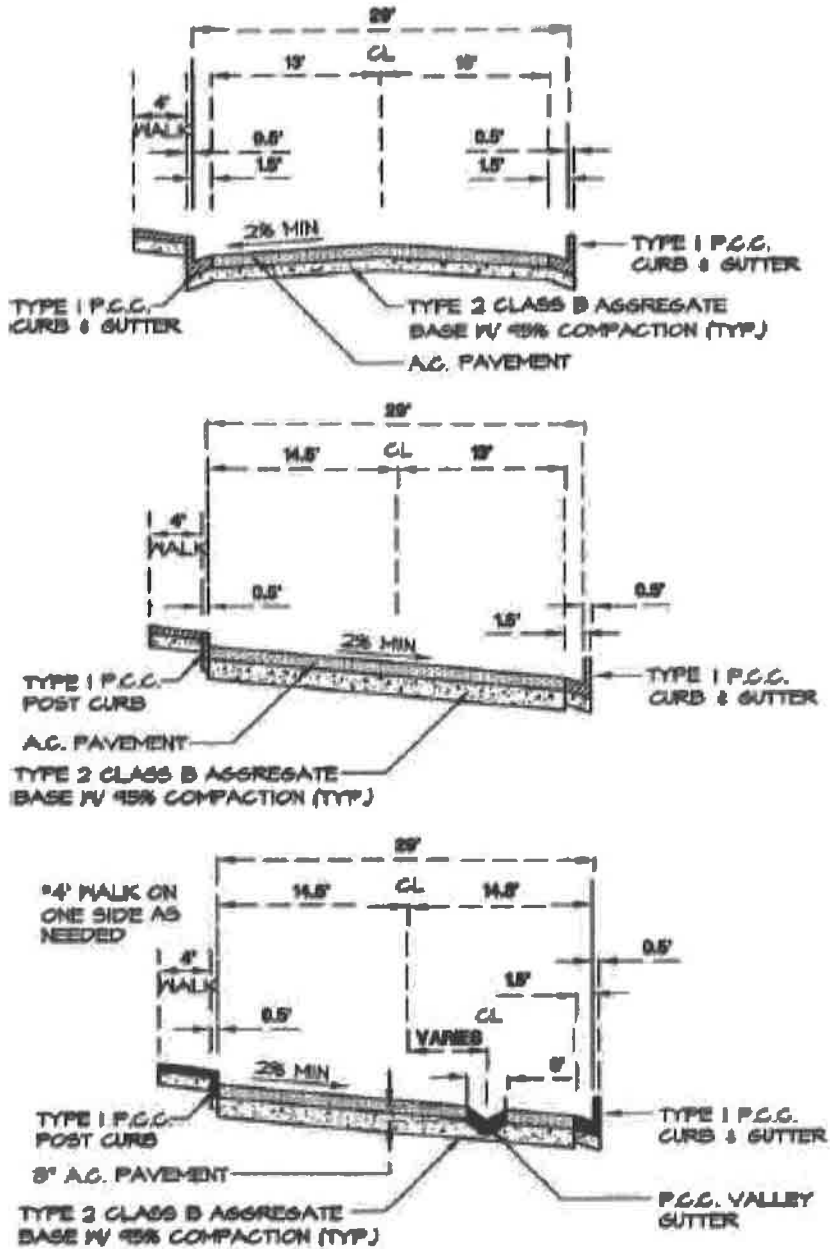
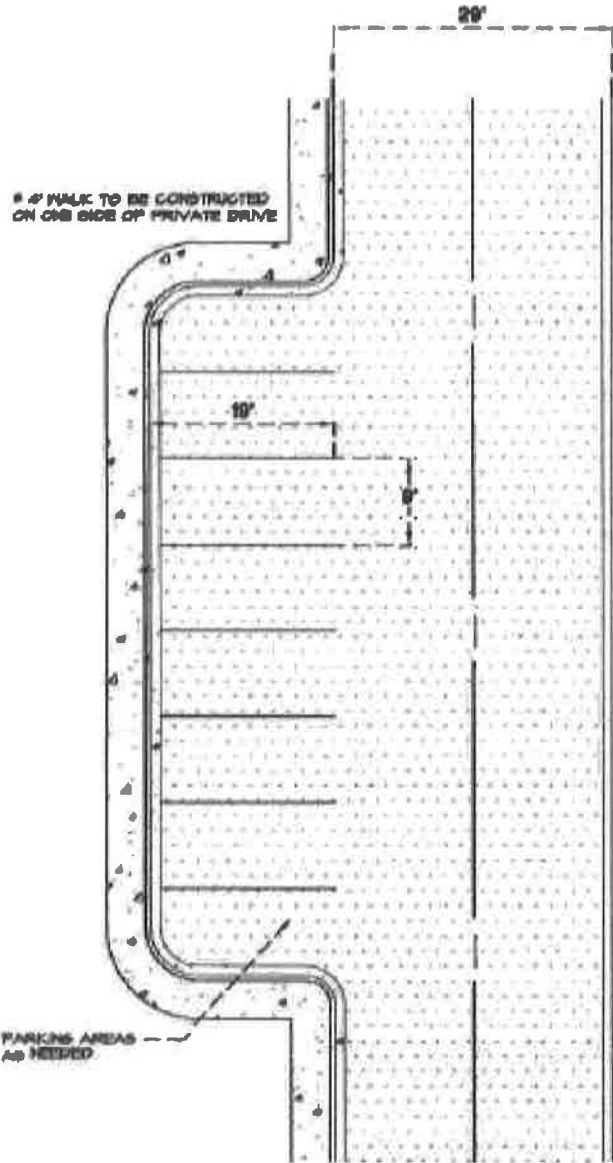
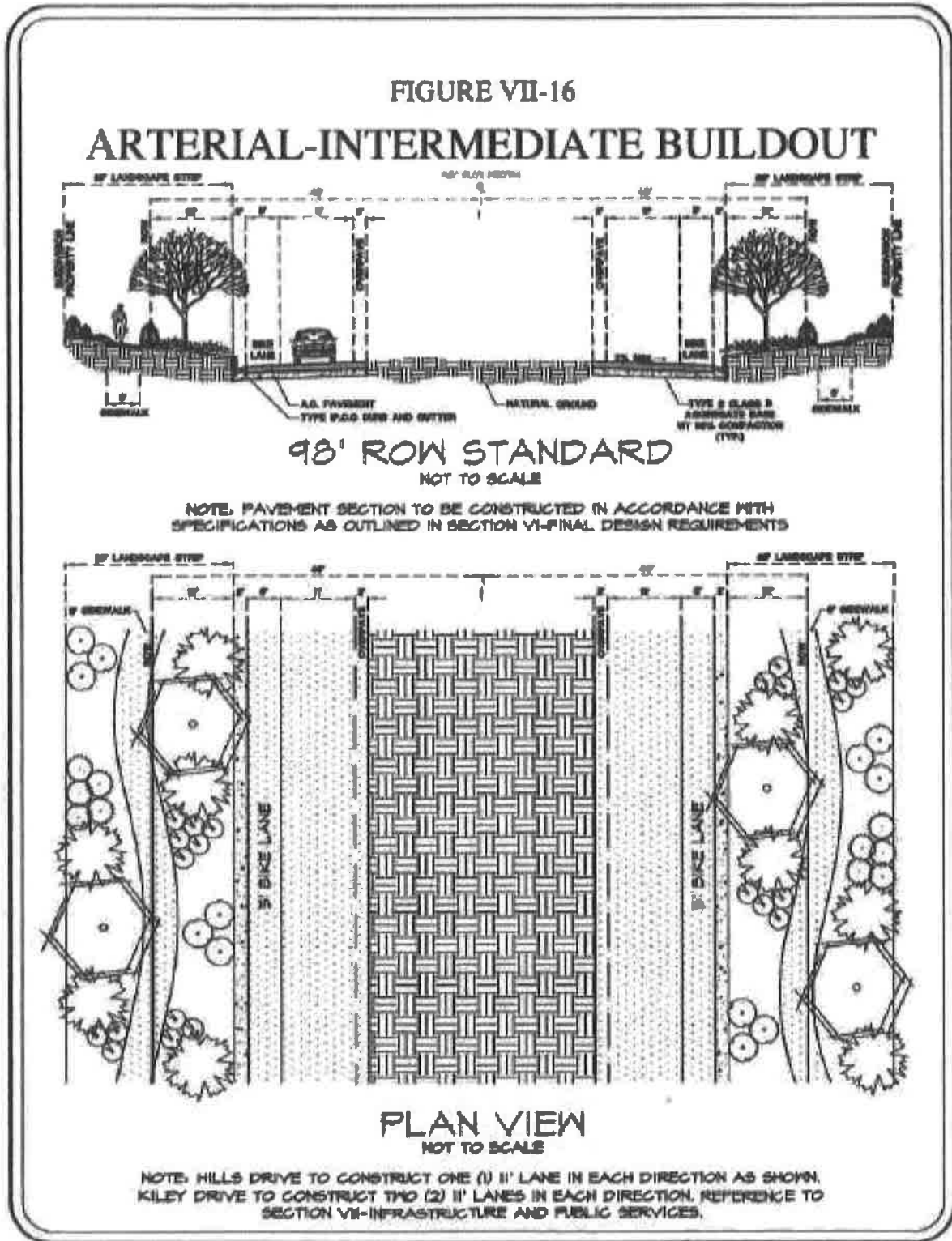


FIGURE VII - 15
PRIVATE DRIVE - PLAN VIEW





FIRE ACCESS AND PROTECTION

- Fire hydrants will be located not more than 300' apart.
- Secondary access to the *Pioneer Meadows* site will be constructed either from the north, tying into La Posada Drive, or from the west through the Kiley property to Pyramid Highway. The access road shall be twenty feet (20') in width and be improved with six inches (6") of type II aggregate compacted to 95% relative density. The secondary access road will be constructed concurrently with the construction of the "Ditch Crossing" on the arterial (Hills Drive), or prior to any stockpile of combustible materials and/or framing of any structures located on the north side of the crossing.
- Temporary secondary access from Vista Boulevard will be provided for the thirty (30) acre multi-family and the twenty-five (25) acre commercial sites located along the southern boundary line of *Pioneer Meadows*. This access will be constructed prior to the construction of the access road described above and prior to the framing of any structures located on the sites.
- Parking shall not be allowed on private streets having a width of twenty-nine feet (29').
- Gated Communities shall have roadway security gates that are operable by a device that senses emergency vehicle strobe lights. The sensing device must be compatible with the equipment used by the City of Sparks' Traffic Division. The roadway security gates shall allow fire equipment to pass safely through the roadway by fully opening upon sensing the emergency vehicle strobe light. They shall be able to open manually in the event of a power failure and shall meet the approval of the Fire Marshal.
- Any structures within the cluster home sites that are located more than 150' from fire apparatus access (as measured by an approved route around the exterior of the structure) shall be protected by an approved NFPA 13D fire sprinkler system to the satisfaction of the Fire Marshal.
- The developer must comply with any fire sprinkler ordinance in effect at the time.
- During construction phasing, fire apparatus access roads shall be designed and maintained to support the imposed loads of fire apparatus and shall provide an all-weather driving surface. When the all-weather driving surface is not asphalt or concrete, it shall meet the specifications set forth in Title 14 of the City of Sparks' Municipal Code.
- The developer shall support increases in the NSSOI Public Facility fee for fire apparatus to adequately serve the development. Alternatively, if the fee is not increased, the developer will participate in discussions of alternative financing methods.
- No wood framing will be allowed on site until fire department access roads and fire hydrants meet the satisfaction and approval of the Fire Marshal.

AIR QUALITY

The primary sources of carbon monoxides are motor vehicles and wood burning stoves. The City of Sparks has banned the use of wood stoves in the area. All new homes built in this development will not be equipped with any type of wood burning apparatus. This ban on wood burning devices will help minimize the production of carbon monoxides produced by the development of this subdivision.

Prior to the commencement of construction of each phase or partial phase of this project, a Dust Control Plan will be submitted to the Air Quality Management Division of the Washoe County District Health Department for review and approval. All construction shall be conducted in accordance with the conditions placed upon the approved Dust Control Plan, as prescribed in Section 040.030(A) of the Washoe County District Board of Health Air Quality Regulations.

UTILITIES

Water

~~Sierra Pacific Power Truckee Meadows Water Authority (TMWA) or Westpac~~ will service Pioneer Meadows. Pioneer Meadows will be serviced from Pump Station #1 located in the Vistas. Water rights will be dedicated with each development per TMWA Rule 7.

~~Pioneer Meadows will require approximately 1,115 acre-feet of Water Rights. This figure includes a drought factor of ten percent (10%).~~

Water Rights Calculations

Single Family

$$(6,000 / 1,000 \times 0.035 + 0.3) \times 720 = 367.20 \text{ Acre-feet}$$

Common Area

$$28 \text{ Acres} \times 3.41 = 95.48 \text{ Acre-feet}$$

Commercial Area

$$109 \text{ Acres} \times 365 \times 307 = 122.14 \text{ Acre-feet}$$

100,000

Apartments

$$1,980 \text{ Units} \times 0.19 = 376.20 \text{ Acre-feet}$$

BP

$$47 \text{ Acres} \times 365 \times 307 = 52.67 \text{ Acre-feet}$$

100,000

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Total _____ = 1,013.69 Acre-feet

Drought calculation = $1,013.69 \times 1.10$ _____ = 1,115.06 Acre-feet

Gas and Electric

All gas lines and underground electric service will be extended to the development with adequate capacity to service *Pioneer Meadows*.

Telephone and Cable Television

Telephone service will be provided by Nevada Bell AT&T and cable television will be provided by TCL of Nevada Charter Communications.

Solid Waste

Solid waste materials will be removed by a disposal company. All residential developments will have mobile toters as a mandatory requirement.

Trash, dumpster and/or compactor enclosures are required for MF15, GC and BP. The enclosures will be compatible with the architectural scheme of the buildings and development.

SECTION VIII
COVENANTS, CONDITIONS AND
RESTRICTIONS

Development Standards Handbook for Pioneer Meadows
Section VIII-Covenants, Conditions, and Restrictions

COVENANTS, CONDITIONS AND RESTRICTIONS

Each residential and commercial development within *Pioneer Meadows* shall be subject to Covenants, Conditions and Restrictions (CC&R's). CC&R's will be developed prior to or concurrent with the development of the first final map for any division of land within *Pioneer Meadows*. CC&R's provided for *Pioneer Meadows* shall be consistent with this Handbook. The following describes the method with which these CC&R's will be developed for each applicable development area:

RD10A, SFR4, and SFR10

RD10A, SFR4, and SFR10 development areas shall be governed by CC&R's which, among other things, restrict certain activities on the building lot (e.g. commercial activities and number and types of pets) and impose architectural and landscaping guidelines and controls. The CC&R's shall conform with or impose greater restrictions than currently applicable ordinances and regulations. RD10A and SFR4 shall be limited expense liability planned communities pursuant to Nevada Revised Statute § 116.1203 and the CC&R's shall expressly restrict annual average liability for common expenses of all units as provided in Nevada Revised Statute § 116.1203.

The CC&R's shall also authorize the creation of a Homeowners' Association for the management and maintenance of the Common Area. The Homeowners' Association shall be organized as a Nevada nonprofit corporation. In order to ensure proper and continued landscape maintenance and an aesthetically pleasing corridor for ingress and egress to the development, the Homeowners' Association shall maintain the landscaping along the arterials and collector streets, which are located in the City of Sparks' right-of-way. Owners of GC, BP or MF15 property within *Pioneer Meadows* shall be required to enter into maintenance/license agreements with the Homeowners' Association whereby the Homeowners' Association shall manage and maintain such landscaping, at the cost of the GC, BP or MF15 property owner.

The CC&R's shall also establish an architectural control committee which shall evaluate and approve or disapprove architectural or landscaping improvements, additions or modifications to the lot or structures located thereon.

The builder shall provide incentives for rear yard landscaping (with a twelve (12) month implementation from the close of escrow) for the RD10A zoning classification.

GC and BP

Each development with the designation GC or BP shall have an exclusive set of CC&R's which shall control the architectural standards for such development and impose landscaping and maintenance obligations. All GC and BP areas shall be required to enter into the maintenance/license agreements with the Homeowners' Association as described above.

MF15 and MF15A

The MF15 and MF15A development areas shall also be subject to CC&R's which, among other things, restrict certain activities on the building lot (e.g. commercial

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activities and number and types of pets) and impose architectural and landscaping guidelines and controls. Like GC and BP development areas, MF15 property owners shall be required to enter into maintenance/license agreements with the Homeowners' Association as described above.

The MF15 and MF15A property owners shall be granted nonexclusive licenses to use the common areas of the Homeowners' Association. The CC&R's shall provide that MF15 and MF15A property owners be assessed fees based upon the number of individual units constructed on the building lot, which shall be payable to the Homeowners' Association, to defer the cost of common area maintenance.

SECTION IX
APPLICATION

Development Standards Handbook for Pioneer Meadows
Section IX-Application

SITE NARRATIVE

Pioneer Meadows is located within the Northern Sparks Area. Pioneer Meadows exists in the City of Sparks and was previously approved as a residential community in July 1999. The original Pioneer Meadows Planned Development Handbook was amended to allow for the potential development of a municipal golf course and was approved in March of 2004. Also included in this amendment were some "housekeeping" changes regarding access, circulation and fencing.

The current Pioneer Meadows Handbook (MP06005) was completed by request of and in cooperation with the City of Sparks on behalf of Pioneer Meadows, LLC. The current Pioneer Meadows Handbook applies to Pioneer Meadows in its entirety (includes areas that were previously adopted as part of the original Master Plan).

The goals of the master amendment #~~32~~ are as follows:

- Clarify and correct inconsistencies with previous master plan to better reflect the City of Sparks Master Plan goals
- Incorporate the City of Sparks design and review requirements input to meet city requirements and planning goals
- Modify the land use classification locations to reflect site conditions

~~Modify to the area infrastructure plan to maximize efficiency.~~

The Pioneer Meadows Master Plan Amendment #~~32~~. Master Plan Amendment #~~32~~ includes the following acreages as defined in the Master Plan Handbook: *acreages are approximate*

102 acres of Business Park (BP-A & BP-B)

Twenty-five (25) acres of General Commercial (GC)

Ninety-one (91) acres of Multi-Family (MC15) and Multi-Family (MF-15A fifteen detached dwelling units per acre (du/ac)

Sixty (60) acres of Single Family (RD10) cluster development up to ten (10) du/ac

~~197.8~~208.4 acres of Single Family (SF4) four (4) du/ac

~~146.7~~ acres of Common Area (CA)

~~One (1) eight and one half (8.5) acre school site~~

~~Three~~Two parks of varying size (totaling ~~seven and one-half~~nine ~~(97.5)~~ acres)

See attached Table II-1 for comparison between Master Plan Amendment #1, #2, #3 and NSSOI Plan

The property is surrounded by the following residential developments: Stonebrook to the north, Wingfield Springs to the east and south and Kiley Ranch North to the West. The major access to Pioneer Meadows is from Vista Boulevard. Vista Blvd delineates the southern border of the Pioneer Meadows and the northern border of Southern Wingfield Springs.

The Pioneer Meadows is a mixed-use neighborhood that integrates business parks, general commercial, single and multi-family homes, ~~a school~~, common area, open space and improved public parks. There is a mix of densities and units within the neighborhoods from single-family detached housing on single lots to neo-traditional clustered single-family home sites and zero lot line home sites. This variety of home products allow Pioneer Meadows to appeal to multiple markets and increase diversity with Sparks.

